

Public Document Pack



PLANNING COMMITTEE

Tuesday, 15th October, 2019 at 8.00 pm
Venue: Conference Room
Civic Centre, Silver Street,
Enfield EN1 3XA

Contact: Jane Creer / Metin Halil
Committee Administrator
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PLEASE NOTE MEETING TIME

MEMBERS

Councillors : Mahmut Aksanoglu (Chair), Sinan Boztas (Vice-Chair),
Mahym Bedekova, Chris Bond, Elif Erbil, Ahmet Hasan, Tim Leaver, Hass Yusuf,
Michael Rye OBE, Jim Steven and Maria Alexandrou

N.B. Any member of the public interested in attending the meeting should ensure that they arrive promptly at 7:45pm
Please note that if the capacity of the room is reached, entry may not be permitted. Public seating will be available on a first come first served basis.

Involved parties may request to make a deputation to the Committee by contacting the committee administrator before 12:00 noon on 14/10/19

AGENDA – PART 1

- 1. WELCOME AND APOLOGIES FOR ABSENCE**
- 2. DECLARATION OF INTEREST**

Members of the Planning Committee are invited to identify any disclosable pecuniary, other pecuniary or non-pecuniary interests relevant to items on the agenda.

- 3. MINUTES OF THE PLANNING COMMITTEE HELD ON TUESDAY 17 SEPTEMBER 2019 (Pages 1 - 2)**

To receive the minutes of the Planning Committee meeting held on Tuesday 17 September 2019.

- 4. REPORT OF THE HEAD OF PLANNING (REPORT NO.120) (Pages 3 - 4)**

To receive the covering report of the Head of Planning.

5. **19/02830/FUL - 39A CAMLET WAY, BARNET, EN4 0LJ** (Pages 5 - 48)

RECOMMENDATION: Approval subject to conditions

WARD: Cockfosters

6. **19/02435/HOU - 47 EVERSLEY PARK ROAD, LONDON N21 1JJ** (Pages 49 - 66)

RECOMMENDATION: Refusal

WARD: Southgate

7. **19/00986/FUL - SINCLAIR'S LAUNDRY SITE, 199 BRETtenham ROAD, LONDON N18 2HE** (Pages 67 - 100)

RECOMMENDATION: Approval subject to conditions and S106 Agreement

WARD: Edmonton Green

8. **19/00201/FUL - 465-469 GREEN LANES, LONDON N13 4BS** (Pages 101 - 144)

RECOMMENDATION: Approval subject to conditions and S106 Agreement

WARD: Winchmore Hill

9. **PLANNING PANEL**

To receive a verbal update on proposals to hold a Planning Panel meeting.

PLANNING COMMITTEE - 17.9.2019**MINUTES OF THE MEETING OF THE PLANNING COMMITTEE
HELD ON TUESDAY, 17 SEPTEMBER 2019****COUNCILLORS**

PRESENT Mahmut Aksanoglu, Sinan Boztas, Mahym Bedekova, Chris Bond, Elif Erbil, Ahmet Hasan, Tim Leaver, Hass Yusuf, Michael Rye OBE, Jim Steven and Maria Alexandrou

ABSENT

OFFICERS: Vincent Lacovara (Head of Planning), Andy Higham (Head of Development Management), Sharon Davidson (Planning Decisions Manager), David Gittens (Planning Decisions Manager), Dominic Millen (Group Leader Transportation) and John Hood (Legal Services) Jane Creer (Secretary)

Also Attending: Dennis Stacey (Chair, Conservation Advisory Group)
17 members of the public, applicant and agent representatives

222**WELCOME AND APOLOGIES FOR ABSENCE**

Councillor Aksanoglu, Chair, welcomed all attendees.

Apologies for lateness were received from Councillor Erbil.

223**DECLARATION OF INTEREST**

NOTED there were no declarations of interest.

224**MINUTES OF THE PLANNING COMMITTEE HELD ON TUESDAY 27
AUGUST 2019**

AGREED that the minutes of the Planning Committee meeting held on Tuesday 27 August 2019 were agreed as a correct record.

225**REPORT OF THE HEAD OF PLANNING (REPORT NO.95)**

RECEIVED the report of the Head of Planning.

226**19/02097/FUL - CHASE FARM HOSPITAL, THE RIDGEWAY, ENFIELD,
EN2 8JL**

PLANNING COMMITTEE - 17.9.2019

NOTED

1. The introduction by David Gittens, Planning Decisions Manager, clarifying the proposals and highlighting the key issues.
2. Councillor Elif Erbil arrived at the meeting and, having missed the beginning of the item, would not be permitted to vote on this application.
3. The deputation of Marc Lewis, Finance Director, Wren Academy in support of the application.
4. Members' debate and questions responded to by officers.
5. The support of the majority of the committee for the officers' recommendation: 7 votes for and 2 abstentions.

AGREED that planning permission be granted, subject to the conditions set out in the report.

MUNICIPAL YEAR 2019/2020 - REPORT NO 120

COMMITTEE:
 PLANNING COMMITTEE
 15.10.2019

AGENDA - PART 1	ITEM 4
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SUBJECT -

MISCELLANEOUS MATTERS

REPORT OF:
 Head of Planning

Contact Officer:
 Planning Decisions Manager
 David Gittens Tel: 020 8132 0870
 Claire Williams Tel: 020 8132 0869

4.1 APPLICATIONS DEALT WITH UNDER DELEGATED POWERS INF

4.1.1 In accordance with delegated powers, 301 applications were determined between 02/09/2019 and 02/10/2019, of which 224 were granted and 77 refused.

4.1.2 A Schedule of Decisions is available in the Members' Library.

Background Papers

To be found on files indicated in Schedule.

4.2 PLANNING APPLICATIONS AND APPLICATIONS TO DISPLAY ADVERTISEMENTS DEC

On the Schedules attached to this report I set out my recommendations in respect of planning applications and applications to display advertisements. I also set out in respect of each application a summary of any representations received and any later observations will be reported verbally at your meeting.

Background Papers

- (1) Section 70 of the Town and Country Planning Act 1990 states that the Local Planning Authority shall have regard to the provisions of the development plan, so far as material to the application, and to any other material considerations. Section 54A of that Act, as inserted by the Planning and Compensation Act 1991, states that where in making any determination under the Planning Acts, regard is to be had to the development, the determination shall be made in accordance with the plan unless the material considerations indicate otherwise. The development plan for the London Borough of Enfield is the London Plan (March 2015), the Core Strategy (2010) and the Development Management Document (2014) together with other supplementary documents identified in the individual reports.
- (2) Other background papers are those contained within the file, the reference number of which is given in the heading to each application.

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 15th October 2019

Report of
Head Of Planning

Contact Officer:
Andy Higham
David Gittens
Joseph McKee

Ward: Cockfosters

Ref: 19/02830/FUL

Category: Full Application

LOCATION: 39A Camlet Way, Barnet, EN4 0LJ

PROPOSAL: Demolition of existing 1no. 4-bedroom dwelling (C3) and erection of 4no. 4-bedroom (8 person) houses with basement level accommodation and associated works.

Applicant Name & Address:

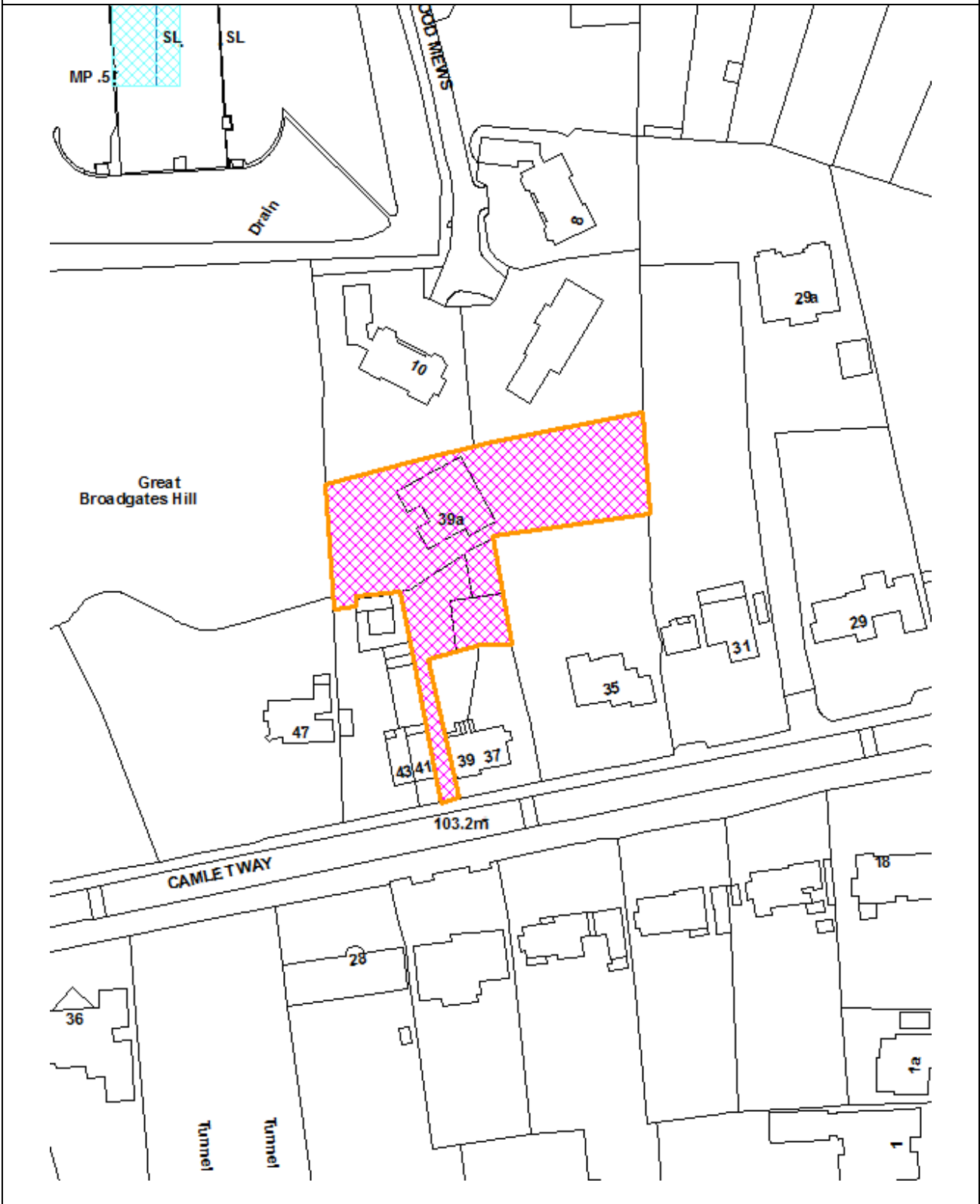
Mrs Sai Taurah
Offshore Incorporations Centre
Road Town
Totola
PO Box 957
saitaurah@gmail.com

Agent Name & Address:

Mr Alan Cox
Alan Cox Associates
224a High Street
Barnet
EN5 5S
alan@coxassociates.co.uk

RECOMMENDATION: That the Head of Development Management/Planning Decisions Manager be authorised to **GRANT** planning permission subject to the conditions.

Ref: 19/02830/FUL LOCATION: 39A Camlet Way, Barnet, EN4 0LJ,



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Scale 1:1250

North



1. Note for Members

- 1.1 Although a planning application for this type of development could be determined by planning officers under delegated authority, the application is being reported to the Planning Committee for determination at the request of Councillor Hayward who considers that the proposed development has not adequately addressed the previous reasons for refusal.

2 Recommendation / Conditions

- 2.1 That the Head of Development Management/Planning Decisions Manager be authorised to GRANT planning permission subject to the following conditions:
1. Time Limit
The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.
Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.
 2. Approved Plans
The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.
Reason: For the avoidance of doubt and in the interests of proper planning.
 1. Materials
Prior to the commencement of development, details of all materials to be used on all external finishes, shall be submitted to and approved in writing by the Local Planning Authority. The development shall be constructed in accordance with the approved details.
Reason: To ensure a satisfactory external appearance.
 4. Window reveal depth
No above ground works shall commence until detailed drawings at a scale of a maximum 1:20 detailing the proposed window reveal depths have been submitted to and approved in writing by the Local Planning

Authority. The development shall be constructed in accordance with the approved details.

Reason: To ensure a satisfactory external appearance.

5. Hard Surfaces

No above ground works shall commence until details of the surfacing materials to be used within the development including footpaths, access roads and parking areas and road markings have been submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure a satisfactory visual appearance and in the interests of highway safety

6. Refuse/recycling storage

The development shall not be occupied until details of bin storage enclosures are submitted to and approved by the Local Planning Authority

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

7. Cycle Parking

The cycle parking facilities shall be provided in accordance with the approved details, as shown on drawing no's. 479318-3 and 479318-9, before the development is occupied.

Reason: To ensure the provision of cycle parking in line with the Council's adopted standards.

8. Energy Performance Certificate

Following practical completion of works a final Energy Performance Certificate shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

9. Final Sustainable Drainage Strategy

Notwithstanding the details set out in the submitted Preliminary Drainage Strategy (19064/SuDS_R01/RS), September 2019, prior to the commencement of any construction work, details of the final Sustainable Drainage Strategy shall be submitted to and approved in writing by the Local Planning Authority and must conform with the Landscaping Strategy. The details shall include:

- a) Sizes, storage volumes, cross-sections, long-sections (where appropriate) and specifications of all the source control SuDS measures including green roofs, permeable paving and rain gardens
- b) Final sizes, storage volumes, invert levels, cross-sections and specifications of all site control SuDS measures including the detention basin and underground tank. Include calculations demonstrating functionality where relevant (including area in m² draining into these features)

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF

10. Groundwater Flood Risk Assessment

Prior to the commencement of any construction work, details of the groundwater level shall be submitted to and approved in writing by the Local Planning Authority. The details shall include:

- a) Photos and a level to the depth of the groundwater table
- b) Measurement from the invert of proposed basement to the water table

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF

11. Suds Implementation Assurance

Prior to occupation of the development, a Verification Report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:

- a) As built drawings of the sustainable drainage systems including level information (if appropriate)
- b) Photographs of the completed sustainable drainage systems
- c) Any relevant certificates from manufacturers/ suppliers of any drainage features
- d) A confirmation statement of the above signed by a chartered engineer

Reasons: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy, DMD 61, and Policies 5.12 & 5.13 of the London Plan and the NPPF

12. Considerate Constructors

The development shall not commence until an undertaking to meet with best practice under the Considerate Constructors Scheme and achieve formal certification has been submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not adversely impact on the surrounding area and to minimise disruption to neighbouring properties.

13. Construction Methodology

The development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:

- a) arrangements for wheel cleaning;
- b) arrangements for the storage of materials;
- c) hours of work;
- d) arrangements for the securing of the site during construction;
- e) the arrangement for the parking of contractors' vehicles clear of the highway;
- f) The siting and design of any ancillary structures; and
- g) A construction management plan written in accordance with the 'Mayor of
- h) London's supplementary planning guidance 'The Control of Dust and Emissions During Construction and Demolition' detailing how dust and emissions will be managed during demolition and construction work.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

14. Site Waste Management Plan

Notwithstanding the approved documents, the development shall not commence until a revised Site Waste Management Plan has been submitted to and approved in writing by the Local Planning Authority. The plan should include as a minimum:

- a) Target benchmarks for resource efficiency set in accordance with best practice
- b) Procedures and commitments to minimize non-hazardous construction waste at design stage. Specify waste minimisation actions relating to at least 3 waste groups and support them by appropriate monitoring of waste.
- c) Procedures for minimising hazardous waste
- d) Monitoring, measuring and reporting of hazardous and non-hazardous site waste production according to the defined waste groups (according to the waste streams generated by the scope of the works)

- e) Procedures and commitments to sort and divert waste from landfill in accordance with the waste hierarchy (reduce; reuse; recycle; recover) according to the defined waste groups

In addition, no less than 85% by weight or by volume of non-hazardous construction, excavation and demolition waste generated by the development has been diverted from landfill

Reason: To maximise the amount of waste diverted from landfill consistent with the waste hierarchy and strategic targets set by Policy DMD57 of the Development Management Document and Policies 5.17, 5.18, 5.19, 5.20 of the London Plan.

15. Clearance of vegetation during bird nesting

No areas of hedges, scrub or similar vegetation where birds may nest shall be cleared outside of the bird nesting season (March-August inclusive). Should clearance during the bird-nesting season be unavoidable, a suitably qualified ecologist shall assess the areas to be removed prior to clearance, and if any active nests are recorded then no further works shall take place until all young have fledged the nest.

Reason: To ensure that wildlife is not adversely impacted by the development, in accordance with policy CP36 of the Core Strategy and the National Planning Policy Framework.

16. Method of enclosure

The site shall not be occupied until detail of the means of enclosure is submitted to and approved in writing by the Local Planning Authority. The means of enclosure shall be erected in accordance with the approved details before the development is occupied.

Reason: To ensure satisfactory appearance and safeguard the privacy, amenity and safety of adjoining occupiers and the public and in the interests of highway safety.

17. Vehicular Parking Compliance Condition

The parking area forming part of the development shall only be used for the parking of private motor vehicles and shall not be used for any other purpose.

Reason: To ensure that the development complies with Development Plan Policies and to prevent the introduction of activity which would be detrimental to amenity.

18. Removal of Permitted Development Rights

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) or any statutory instrument revoking and re-enacting or modifying that Order, no development under the following classes shall take place without the prior written consent of the Local Planning Authority:

- a. Article 3 and Schedule 2, Part 1 Classes A, B, C, D, E and F

Reason: In order to protect the amenities of the adjoining occupiers, to ensure adequate amenity space is retained for each of the dwellings in accordance with adopted Policy, and to maintain a satisfactory appearance to the development.

- 19 The development shall not commence until the applicant submits details of a scheme of ecological enhancement methods inclusive of bat and bird boxes to be designed into the development.

Reason: To ensure development is in compliance with Policy DMD79 of the Enfield Development Management Document (2014).

3. Executive Summary

- 3.1 The application reflects advice provided at a pre-application (18/03224/PREAPP) stage, which was sought by the applicant post the refusal of 2no. planning applications affecting the re-development of the site; ref. 16/00877/FUL and 17/04406/FUL. By reason of development representing a contemporary and high-quality approach to architecture which of a low-rise scale, the development is considered an appropriate subservient back land re-development of the site.
- 3.2 The LPA considers the development to represent high-quality residential accommodation by reason of gross internal areas of all 4no. proposed units, exceeding by a significant amount, minimum floorspace standards as outlined within the DCLG's Technical Housing Standards (2015). Indeed, all units benefit from private amenity space which exceeds Local Plan requirements.
- 3.3 The site is somewhat constrained in that the topography of the land falls away to the north, therefore any development on site of an excessive scale, could appear overbearing and/or have an unacceptable adverse impact to the residential amenity of the occupiers of no's 9 and 10 Alderwood Mews (existing dwellings north of the application site). Indeed, the sole access to the site is off an existing

lane that runs between 39 and 41 Camlet Way measuring 3.86m in width and 32m in length. In view of the relatively small scale of the proposed development; which would not result in any significant increase to vehicular traffic, the Local Planning Authority (LPA) together with the Highways officers are satisfied that the reuse of this existing access, is acceptable. Indeed, a private arrangement for refuse/recycling collection is proposed by the applicant. This approach is only acceptable in particular site-specific circumstances. For reasons outlined within the body of this report, the approach is on balance acceptable noting the site constraints.

- 3.4 In order to ensure the proposed development satisfies in full, concern surrounding impact to neighbouring residential amenity, a significant difference between this application and those previously refused, is the introduction of basement level accommodation. This has been designed in a way which the LPA is satisfied, all accommodation has adequate levels of daylight/sunlight and privacy yet which results in what appears largely a part-single-storey, part two-storey development. Whilst proposed units; particular *plots 3 and 4* extend somewhat close to the boundary, this is solely at a single-storey level.
- 3.5 Whilst not prominent, the proposed development would be visible in private views from neighbouring residential property, which is normal for most residential development. However, considering the back land nature of the application site, the proposed development would not be highly visible from any public views. This is especially the case noting the flatted development in the process of being built out at no. 35 Camlet Way, which is significantly taller than the proposed development. It should be noted that, in itself, private views of any proposed development do not constitute a material consideration in the determination of any planning application.
- 3.6 As per the London Plan policy 3.13 and guidance in the DMPO (2015), the proposed development constitutes minor development and the LPA would not seek to secure financial obligations in the form of a S106 agreement from the applicant.

4. Site and Surroundings

- 4.1 The application site is located within the Cockfosters Ward in the north-west of the London Borough of Enfield. It is an irregular shaped site fronting the northern side of Camlet Way. The site measures 2204sqm and its northern boundary abuts that of no's 9 and 10 Alderwood Mews, to the east, abuts that of 31 Camlet

Way, to the west, 47 Camlet Way and to the south and south-west, 35 Camlet Way; which hosts a re-developed flatted development in the process of being built out and 37-43 Camlet Way; which front highway. The site is accessed from Camlet Way by a single-lane existing private laneway (32.4m in length) located between 39 and 41 Camlet Way. The site has quite heavy foliage throughout however the site is not subject to any Tree Preservation Orders.

- 4.2 The site as existing hosts 1no. two-storey (4-bed) dwelling within the central northern part of the site which is proposed to be demolished as part of the proposed re-development of the site. There are limited public views into the site noting land proposing to be re-developed is quite set-back from Camlet Way noting the laneway; and that the northern part of the site is located behind existing neighbouring dwellings fronting the northern side of Camlet Way.
- 4.3 The surrounding built context is varied in its age, scale and appearance however the area is an established residential area and is quite suburban in its appearance. The northern site boundary is 45.6m south of the southern boundary of the Hadley Wood designated Conservation Area. Development would not affect any local of statutory listed building.
- 4.4 Two existing vehicle garages are located outside of the red line of the application site that can only be accessed by the lane. These, are understood to be owned by the owners of existing dwellings fronting Camlet Way, south of the application site. These are to be retained and would not be affected by the development.

5 Proposal

- 5.1 The development proposes 4no. (4-bedroom) dwellings of a contemporary appearance over 3-storeys with all units integrating basement, ground and first floor level accommodation.
- 5.2 Plot 1 is a part single-storey part two-storey (plus basement accommodation) detached dwelling located on the central eastern part of the site; close to the shared boundary of the site with no's.37 and 39 Camlet Way and that shared with 35 Camlet Way. Two-storey accommodation is proposed a minimum of 13.61m from the southern shared boundary and 9.15m from the eastern shared boundary of the site. The unit would benefit from 251sqm of private amenity space.
- 5.3 Plot 2 is also a part single-storey part two-storey (plus basement accommodation) detached dwelling located on the north-western part of the site.

The single-storey (northern) side elevation is located 2.84m (at the corner with eastern front elevation) and 1.77m (at the western elevation) from the shared boundary with 10 Alderwood Mews. Two storey accommodation is set back from this boundary by 8.38m. The unit benefits from 245sqm of private amenity space.

- 5.4 Plots 3 and 4 comprises of a semi-detached pair which are also part single-storey part two storey (plus basement accommodation) which are located on the north-eastern part of the site. The single-storey element of the proposal is located 1.45m from the shared northern boundary with no.9 Alderwood Mews. Two storey accommodation is set-back from this shared boundary by 5.18m. Plot 3 benefits from 223sqm of private amenity space and plot 4 benefits from 198sqm of private amenity space.
- 5.5 The proposed development integrates both PV solar panels and green roofs at a first-floor level across all roofs. Each dwelling is served by 2no. vehicular parking spaces and both hard and soft landscaping is proposed across the development site.

6. **Relevant Planning History**

Application site

Reference – 18/03224/PREAPP

Development Description – Proposed redevelopment of site and erection of 4 x residential units.

Decision Type – Officer Level Advice Provided

Reference – 17/04406/FUL

Development Description – Redevelopment of site and erection of 2 x 3 bed single family dwellings and a block of 4 self contained flats comprising 4 x 3 bed with associated parking and landscaping.

Decision Level – Delegated

Decision Type – Refused

Decision Date – 18.12.2017

Reference – 16/00877/FUL

Development Description - Redevelopment of site and erection of 2 detached 5 bed single family dwellings together with garage and raised terraces.

Decision Level – Delegated

Decision Type - Refused

Decision Date – 19.05.2016

31 Camlet Way

Reference – 17/02071/FUL

Development Description – Redevelopment of site by the erection of a detached 2-storey, 6-bed dwelling house including rooms in roof, basement level with incorporating swimming pool, garage at front and associated landscaping.

Decision Level – Delegated

Decision Date – 10.07.2017

35 Camlet Way

Reference – 16/05740/FUL

Development Description – Minor material amendment to 14/02622/FUL to allow increase in building height by 700mm, increase of parking spaces, alterations to size of ground floor apartments, elevations to include feature windows, brick quoin and stone copping details, glazed balconies, removal of railings to side elevation, rooflights to replace dormer windows to side together with alterations to fenestration and other associated works.

Decision Level – Delegated

Decision Type – Granted

Decision Date – 07.02.2017

Reference – 16/00201/FUL

Development Description – Minor Material amendment to 14/02622/FUL to allow increase in building height by 700mm, increase in parking spaces and loss of residential floor space on basement level, amendments to size of ground floor apartments, alterations to elevations to include additional feature windows on gables, brick Quoin and stone coping details, railing on balconies replaced with glazing, brick/stone detailing on entrance to replace railings, splayed window detailing, railings removed on side elevation, insertion of 4 windows to side elevation, rooflights to replace dormer windows on side elevation, glass lantern added on roof to hide lift overhang, amended dormer detail, window proportions, front door detail to include double doors and chimney design.

Decision Level – Delegated

Decision Type – Granted

Decision Date – 13.04.2016

Reference – 14/02622/FUL

Development Description – Redevelopment of the site to provide 8 residential apartments (Class C3)

Decision Level – Granted
Decision Type – Delegated
Decision Date – 27.03.2015

7. Consultation

7.1 Neighbour Notification

7.2 13no. surrounding properties were notified of development (consultation period 13.09.2019). At the time of writing the report, **eight** objections were received from residents. A summary of the comments made within representations received is below:

- Inadequate access arrangement;
- Increase in traffic;
- Insufficient vehicular parking;
- Refuse collection;
- Out of character with surrounding dwellings;
- Overdevelopment of site;
- Right of way of access respectively in ownership of 37, 39 and 41 Camlet Way;
- Topography of site;
- Overlooking impact;
- Loss of privacy;
- Lack of tree screening between shared boundary; 39A and 31 Camlet Way;
- Proximity to northern boundary; shared with no's. 9 and 10 Alterwood Mews;
- Flood risk increase;
- Noise impact of intensified use;
- Excessive scale/massing
- More open space needed on development
- Impact to trees

7.3 Statutory and Non-Statutory Consultees:

7.4 Local Highways Authority: No objection. Comments integrated into body of report.

7.5 Borough Urban Design Officer – No objection. Comments integrated into body of report.

- 7.6 London Fire Brigade and Emergency Services – No comment
- 7.7 SUDS – No objection subject to appropriate conditioning requiring ground water flood risk assessment to ensure safety of basement level accommodation.
- 7.8 Building Regulations – Agreement with the applicant’s consultant.

8.0 Assessment

The application is assessed in the context of the following policies:

Relevant Planning Policies

London Plan (2016)

- 3.3 - Increasing housing supply
- 3.4 - Optimising Housing potential
- 3.5 - Quality and design of housing developments
- 3.9- Mixed and Balanced Communities
- 3.14 - Existing Housing Stock
- 5.1 - Climate change mitigation
- 5.2 - Minimising carbon dioxide emissions
- 5.3 - Sustainable design and construction
- 5.7 - Renewable energy
- 5.13 - Sustainable Drainage
- 5.14 - Water Quality and Wastewater Infrastructure
- 5.15 - Water Use and Supplies
- 5.16 - Waste Self Sufficiency
- 6.9 - Cycling
- 6.10 - Walking
- 6.13 - Parking
- 7.1 - Lifetime Neighbourhoods
- 7.3 - Designing out Crime
- 7.4 - Local Character
- 7.6 - Architecture
- 7.19 - Biodiversity and access to nature
- 7.21 - Trees and Woodland
- 8.3 - Community Infrastructure Levy

Core Strategy (2010)

CP2 - Housing supply and locations for new homes
CP4 - Housing quality
CP5 - Housing types
CP20 - Sustainable energy use and energy infrastructure
CP21 - Delivering sustainable water supply, drainage and sewerage infrastructure
CP22 - Delivering sustainable waste management
CP25 - Pedestrians and cyclists
CP30 - Maintaining and improving the quality of the built and open environment
CP32 - Pollution

Development Management Document (2014)

DMD2 - Affordable Housing for Development of Less than 10 units
DMD3 - Providing a Mix of Different Sized Homes
DMD5 - Residential Conversions
DMD6 - Residential Character
DMD7 - Development of Garden Land
DMD8 - General Standards for New Residential Development
DMD9 - Amenity Space
DMD10 - Distancing
DMD11 - Rear Extensions
DMD14 - Side Extensions
DMD37 - Achieving High Quality and Design-Led Development
DMD45 - Parking Standards
DMD49 - Sustainable Design and Construction Statements
DMD51 - Energy Efficiency Standards
DMD58 - Water Efficiency
DMD61 - Managing Surface Water
DMD68 - Noise
DMD81 - Landscaping
DMD Appendix 7 - London Plan parking and Cycle standards
DMD Appendix 8 - Parking standards (parking dimensions)
DMD Appendix 9 - Road classifications

Other Policy

National Planning Policy Framework (2019)
National Planning Practice Guidance
London Plan (2016)

Mayor of London Housing SPG (March 2016)
DCLG Technical Housing Standards (2015)
Enfield Strategic Housing Market Assessment Update (2015)
Community Infrastructure Levy Regulations 2010
LBE S106 SPD (November 2016)

- 8.1 The remainder of this report sets out the analysis of the issues that arise from the proposal assessed against National, London-wide and adopted local planning policies.
- 8.2 The following areas have been assessed with this report:
- Background History on Site
 - Principle of Development/Land Use Implications
 - Density
 - Dwelling Mix
 - Impact on Character, Design, Scale and Height Considerations
 - Quality of Accommodation
 - Highways, Access and Refuse
 - Trees and Landscaping
 - Sustainable Drainage
 - Ecology
 - Energy
 - Water Consumption

Background history on site:

- 8.3 The applicant sought pre-application advice (18/03224/PREAPP) stage, which was sought by the applicant post the refusal of 2no. planning applications affecting the re-development of the site; ref. 16/00877/FUL and 17/04406/FUL. The pre-application scheme was somewhat limited in detail however the proposal somewhat reflected that as proposed in scale and layout. Officers were generally quite positive about the development subject to technical information accompanying any future application and concluded the principle of “backland” development was likely acceptable noting the presence of the existing dwelling. Officers noted the basement storey design largely resolve the previous overbearing nature of development on site; especially on no’s 9 and 10 Alderwood Mews. It was noted the overall design approach was contemporary and may be appropriate within this withdrawn location away from the public realm.

8.4 Refused application 17/04406/FUL proposed 6no. dwellings on site across 3no. proposed buildings; 2no. (3-bedroom) detached dwellings and 4no. (3-bedroom) self-contained flats within a building on the north-east part of the site. The development proposed 681sqm of residential floorspace across all units. The buildings proposed were two-storey in height with dormer windows within most roof pitches. The approach to architecture was quite a traditional one with hipped- and pitched and part flat roofs. No basement level accommodation was proposed.

8.5 The LPA refused planning permission for the below reasons:

1. *The proposed subdivision of the site and the creation of two houses with garages due to their design, excessive size, scale, building footprint, bulk, massing, layout and proximity to the boundaries would be a dominant, obtrusive and overbearing form of development that would result in demonstrable harm to the open, spacious and suburban character and appearance of the site and area; and fail to respect the pattern of development that characterises the surrounding area. In this regard the development would constitute an unsustainable form of backland development that would be contrary to Policy 3.5 of the London Plan, Policy CP30 of the Core Strategy and Policies DMD6, DMD7, DMD8 and DMD37 of the Development Management Document and the NPPF.*
2. *The proposed development due to the topographical differences between the application site and the land to the north of the site, the proximity of the new dwellings to the northern boundary and its design, siting, excessive size, scale, building footprint, bulk and massing would result in the creation of an obtrusive, overly dominant, overbearing and incongruous form of development prejudicial to the residential amenity of No. 9 Alderwood Mews through a heightened sense of enclosure, oppressive outlook and actual and perceived overlooking to habitable rooms and rear gardens. This would fail to accord with Policy 7.6 of the London Plan, Policy CP30 of the Core Strategy and Policies DMD7, DMD8 and DMD11 of the Development Management Document.*
3. *The proposal, due to the lack of outlook to the north facing bedrooms of flats C1 and C2 would result in a poor outlook, lack of natural light and heightened sense of enclosure to the occupants of these rooms, giving rise to poor living conditions to occupiers of the property, precluding practical use and unable to meet with the reasonable demands of existing*

and future occupiers. The proposal would be contrary to Policy CP4 of the Core Strategy, DMD 6, 8, 9 and 37 of the Development Management Document and Policy 3.5 including accompanying Table 3.3 of the London Plan, the London Housing SPG, the objectives of the NPPF and the Nationally Described Space Standard.

- 8.6 Refused application 16/00877/FUL proposed 2no. (5-bedroom) detached dwellings on site with a total GIA of 868sqm. The dwellings were two-storey with accommodation at a third storey level within the roof form. The approach to architecture was quite traditional with Dutch gable detailing, fenestration patterns reflecting that on the period dwellings surrounding the application site and part hipped-and-pitched part flat roofs.
1. *The proposed subdivision of the site and the creation of two houses with garages due to their design, excessive size, scale, building footprint, bulk, massing, layout and proximity to the boundaries would be a dominant, obtrusive and overbearing form of development that would result in demonstrable harm to the open, spacious and suburban character and appearance of the site and area; and fail to respect the pattern of development that characterises the surrounding area. In this regard the development would constitute an unsustainable form of backland development that would be contrary to Policy 3.5 of the London Plan, Policy CP30 of the Core Strategy and Policies DMD6, DMD7, DMD8 and DMD37 of the Development Management Document and the NPPF.*
 2. *The proposed development due to the topographical differences between the application site and the land to the north of the site, the proximity of the new dwellings and garages to the northern boundary and its design, siting, excessive size, scale, building footprint, bulk and massing would result in the creation of an obtrusive, overly dominant, overbearing and incongruous form of development prejudicial to the residential amenity of No's 9 and 10 Alderwood Mews through a heightened sense of enclosure, oppressive outlook and actual and perceived overlooking to habitable rooms and rear gardens. This would fail to accord with Policy 7.6 of the London Plan, Policy CP30 of the Core Strategy and Policies DMD7, DMD8 and DMD11 of the Development Management Document.*
 3. *Insufficient information has been provided to confirm that there will be adequate provision for the retention and long term survival of the trees covered by a Tree Preservation Order on and adjacent to the site. The trees are prominent and offer valuable amenity within the local landscape*

and the irrevocable damage to these trees caused by the development would be unjustifiable and unacceptable. The proposal would therefore be contrary to the NPPF, NPPG, London Plan policies 7.19 and 7.21, Core Strategy policy CP34 and DMD 80 of the Development Management Document.

4. *A mechanism to secure the affordable housing and education contributions has not been advanced. This is contrary to Policies CP3, CP8 and CP46 of the Core Strategy (2010), Policy DMD2 of the Development Management Document (2014), Policies 3.10, 3.11, 3.12 and 3.13 of the London Plan, the Section 106 SPD and the National Planning Policy Framework (2012).*

Principle of Development/Land Use Implications

- 8.7 Development would be broadly compatible with policies 3.3 and 3.4 of the London Plan (2016) and Core Policies 2 and 5 of the Core Strategy insofar as it provides additions to the Borough's housing stock which actively contributes towards both Borough specific and London-wide strategic housing targets.
- 8.8 However, the position must be qualified in relation to other material considerations including any proposed development being of an appropriate scale, design, density, representing good quality of residential accommodation, highways and access matters, impact to residential amenity, sustainable design and construction etc.
- 8.9 The proposed development involves the demolition of the existing dwelling, and an intensification of the residential use on site. Noting the predominant surrounding use is residential, and the presence of the existing dwelling on site, the principle of development is acceptable so long as development adequately complies with all relevant policy considerations; the primary areas as outlined above.
- 8.10 Development represents an intensification of the residential use of the site however does not propose any change of use. The application therefore does not result in any land-use related implications.

Density

- 8.11 The London Plan Density Matrix (Table 3.2) outlines that where a site has a PTAL level of 0 to 1, high density could be interpreted as developing between 35 and 75 units per hectare. The LPA calculates the development density as somewhere in the region of 18 units per hectare. Development is therefore not considered dense, when compared against the adopted London Plan Density Matrix

Dwelling Mix

- 8.12 The Council commissioned a Strategic Housing Market Assessment (SHMA). This formed part of the Council's evidence base for its Core Strategy, which was examined at Public Inquiry and found to be sound by the Secretary of State and subsequently adopted by the Council (2010).
- 8.13 Policy 3.9 of the London Plan requires a more balanced mix of tenures to be sought in all parts of London with the aim of achieving more mixed and balanced communities. Enfield's DMD 3, Providing a Mix of Different Sized Homes, require a mix of different sized homes.
- 8.14 Core Policy 5 of the Core Strategy (2010) seeks to ensure that "new developments offer a range of housing sizes to meet housing needs" and that the Policy should support the Council's plan for a Borough-wide mix of housing that reflects the needs and level of supply identified in the SHMA (2010). The 'Justification' in support of the Policy 5 of the Core Strategy is instructive, and in paragraphs 5.40 and 5.41 it is noted that the supply-to-need shortage is most acute for larger dwelling types and that is unlikely that the required supply can be met through new build completions. The Policy requires that the Council, over the lifetime of the Core Strategy, plans for a mix of housing that is 80% houses (mainly 3 and 4-beds) and 20% one and two-bed flats.
- 8.15 Policy DMD3 requires an approach to maximising the provision of family housing (3bed+). This is supported by Policy DMD5 (2.a.), which requires the compensatory provision of family accommodation to be provided with the conversion of existing family units.

- 8.16 The development defines minor development and proposes to replace 1no. existing family sized dwelling (defined as 3+ bedrooms) with 4no. 4-bedroom (8-person) dwellings. The dwelling mix is considered acceptable and adequately compliant with the spirit of London and Local Plan relevant policy objectives.

Impact on Character, Design, Scale and Height Considerations

- 8.17 Representations received objected on design of development on below grounds:

- Out of character with surrounding dwellings;
- Overdevelopment of site; and
- Excessive scale/massing.

- 8.18 The application proposes the demolition of the existing dwelling and construction of 3no. buildings - 2no. detached dwellings and 1no. semi-detached pair; 4no dwellings in total.

- 8.19 Previous development on site which was refused was somewhat different to that as proposed in terms of design and appearance.

- 8.20 Application 16/00877/FUL proposed 2no. large detached dwellings of a maximum height of approximately 9.3m to be located within the central part of the site; with *plot 2* being close to the northern boundary of the application site. Dwellings, by reason of their scaling and massing were concluded to result in demonstrable harm to the open, spacious and suburban character and appearance of the site and area.

- 8.21 Application 17/04406/FUL proposed the erection of 3no. buildings comprising 2no. detached dwellings and a building accommodating 4no. (3-bedroom) flats. In its site layout, the development somewhat reflected that as proposed under this current application however the flatted element of the development was proposed further to the east than plots 3 and 4 as proposed.

- 8.22 Dwellings proposed constitute 3-storey accommodation, with all having basement, ground and first floor level accommodation. From front elevations, dwellings appear part single-storey part-two-storey, with lightwells serving basement level accommodation on front elevations. Heights vary slightly across the site, noting the topography however single storey elements measure a maximum 3.42m in height. Two storey elements measure a maximum 6.84m in height. It is only to the rear of dwellings that through excavation, basement level accommodation is opened up and more visible. The development proposes a

contemporary approach to architecture with simplistic single-storey flat roof and parapet elements and feature gables integrating two-storey accommodation.

- 8.23 The LPA is supportive of the contemporary and simplistic approach to design. The Borough Urban Design Officer was consulted about the development and stated no objection to the general design rationale.
- 8.24 With regards to materiality, quite a natural palette is proposed with a light-yellow brick; resembling a London Stock Brick to be used on all elevations, natural slate on pitched roofs and green roofs at a first floor (flat-roof) level, powder coated aluminium for windows, both aluminium and timber to be utilised for doors and corten steel acting as an architectural feature; being integrated into porch areas and with first floor gable windows.
- 8.25 Whilst the surrounding area is suburban and residential in its character, there exists no overwhelming characteristics when thinking about design or scale; particularly when considering more contemporary development granted permission on both 31 and 35 Camlet Way. Generally, development is quite linear; fronting Camlet Way however this pattern is not regimented; noting the existing back-land development on site and the presence of no's. 9-10 Alderwood Mews. Noting this, there exists some degree of flexibility with regards to design rationale within this location. The low-rise nature of the proposed development is sensitive to the topography of site and the surrounding area (impact to neighbouring residential amenity assessed within relevant section of report) and public views of the site are very limited; noting the long access lane separating the site from Camlet Way. For reasons outlined, development is not considered to be at odds with the character of the surrounding area compliant with the outlined relevant policy framework.
- 8.26 Despite the proposed development reflects an increase in residential floorspace in comparison to previously refused schemes, owing to the proposing of basement level accommodation (which didn't form part of any previous application), the applicant is able to provide dwellings with an adequate internal floorspaces but which appear quite low-rise in their scale when viewed from private views from surrounding sites (which doesn't form a material consideration in the determination of a planning application). Indeed, in loose terms, the development somewhat reflects that as proposed under refused application 17/04406/FUL. However, owing to plots 3 and 4 being proposed further west; into the application site; any development close to the edge of the curtilage of the plot, is single storey only. For reasons outlined, development is not considered the overdevelopment of the site.

- 8.27 In conclusion, the proposed scale and design of development are considered acceptable and would integrate acceptably into the surrounding locality and the in compliance with policies DMD6, 8 and 37, CP30 of the Core Strategy and London Plan policies 7.4 and 7.6.

Quality of Accommodation

Representations received objected on design of development on below grounds:

- Lack of open space on development.
- 8.28 DMD 8 of the Enfield Development Management Document (2014), Policy 3.5 of the London Plan (2016), London Plan Housing SPG (2016) and the DCLG's Technical Standards outline minimum space standards for new dwellings and required criteria for new residential accommodation to adhere to. See the below table which outlines dwelling floorspace areas:

Plot No	Bed/Person	DCLG Required Floorspace (sqm)	Proposed Floorspace (sqm)
Plot 1	4-bed/8-person	130	251
Plot 2	4-bed/8-person	130	229
Plot 3	4-bed/8-person	130	251
Plot 4	4-bed/8-person	130	251

- 8.28 The internal floorspace of each dwelling unit would considerably exceed minimum internal floorspace standards for a 4-bedroom (8-person) dwelling over 3-storeys. All bedrooms would exceed 11.5sqm and therefore constitute double bedrooms. All habitable rooms in spite of proposed basement accommodation would have adequate outlook, provision of natural light and adequate levels of privacy.
- 8.29 Each proposed unit would have private amenity space. Policy DMD 9 (Amenity space) provides the Council's external amenity space standards. Standards only provide an average amenity provision guideline amount for a 4-bed (6-person) dwelling. See below the proposed provision:

Plot No	Bed/Person	Required Amenity Space (4-bed, 6-person) (sqm)	Provided Amenity Space (sqm)
Plot 1	4-bed/8-person	9	251
Flat 2	4-bed/8-person	9	245
Flat 3	4-bed/8-person	7	223
Flat 4	4-bed/8-person	5	198

- 8.30 Policy DMD 8 states that new residential development is required to have a well designed layout and Standard 29 of the London Housing SPG states that development should minimise the number of single aspect dwellings; especially units that are north facing.
- 8.31 Each unit has integrated storage areas, functional layouts and adequately sized rooms.
- 8.32 The proposed vegetable patches; integrated into plots 1 and 2, are encouraged however their inclusion doesn't hold much weight in the determination of this application noting it wouldn't be an enforceable condition for these to be maintained as such.
- 8.33 The overall residential offer from a quality of accommodation perspective is acceptable and complies with Policy 3.5 of the London Plan (2016), the London Housing SPG (2016), the DCLG's Technical Standards (2015) and Policies DMD 8 and DMD 9 of the Enfield Development Management Plan (2014).

Impact to Neighbouring Residential Amenity

- 8.34 Representations received which objected on the basis of development's impacts to residential amenity on the following grounds:
- Topography of site;
 - Overlooking impact;
 - Loss of privacy;
 - Lack of tree screening between shared boundary; 39A and 31 Camlet Way;
 - Proximity to northern boundary; shared with no's. 9 and 10 Alderwood Mews;

- Noise impact of intensified use;
- 8.35 Policy 7.6 of the London Plan states that developments should have appropriate regard to their surroundings, and that they improve the environment in terms of residential amenity. Policy CP30 of the Enfield Core Strategy seeks to ensure that new developments are high quality and design-led, having regards to their context. They should help to deliver Core Strategy policy CP9 in supporting community cohesion by promoting attractive, safe, accessible and inclusive neighbourhoods. Policy DMD8 states that new developments should preserve amenity in terms of daylight, sunlight, outlook, privacy, overlooking, noise and disturbance.
- 8.36 The application is accompanied by a daylight and sunlight assessment (dated 20.06.19). which suggests that the most affected neighbouring property is the house at no.9 Alderwood Mews to the north-east of the application site. The daylight and sunlight consultant has undertaken the Vertical Sky Component (VSC) test. BRE Guidelines indicate that for a development to pass the test, an impacted window, with the development in place, should maintain at least 80% of the daylight levels experienced pre-development. All windows on the south-east (rear) elevation of no. 9 Alderwood Mews were tested. It was confirmed that all windows exceed the test by a significant amount; with only two windows; *window A* (ground floor) and *window L* (first floor), experiencing any reduction in daylight as a result of the proposed development. This reduction is calculated at a 0.1% reduction and therefore would be negligible. Impacts of the proposed development to sunlight levels received by windows in the rear elevation of no.9 were also tested. Results show that the proposed development would result in a maximum of 0.06 ratio reduction to sunlight access the most affected windows. This impact is considered less than negligible and compliant with BRE Guidelines.
- 8.37 The daylight/sunlight assessment tested windows within both the south-west (rear) and south east (side) elevation of no.10 Alderwood Mews. All results demonstrate that the impact of the development upon these windows would be minimal.
- 8.38 Policy DMD10 of the Development Management Document outlines that new development is required to maintain minimum distances between buildings; in order to avoid unacceptable adverse impacts to daylight, sunlight and overlooking. The policy outlines a minimum of 22 metres between rear facing windows and recommends the avoiding of side windows unless it can be demonstrated that overlooking and loss of privacy would be insignificant.

- 8.39 A window is proposed within the northern side elevation of plot four at a ground floor level, which is 16.5 metres from nearest windows within the south-eastern rear elevation of no.9 Alderwood Mews. However, noting the presence of the boundary wall and that the window is set-back from the boundary by 7.8 metres, this window would have no adverse impact to neighbouring residential amenity despite the topography of the site. The same window within adjoining plot 3, orientated south, is to be located 27 metres from the rear elevation of the built out flatted development at no.35 Camlet Way. Whilst the submitted proposed site plan (dwg. no. 479318-3) does not integrate on to it either the existing dwelling; or that as approved under application 17/02071/FUL, the closest proposed ground floor windows are 17 metres from the shared boundary and the closest first floor window, is 27 metres away.
- 8.40 Proposed plot 1 is to have integrated, windows on its northern side elevation; which look into the site. They serve the hallway and a bedroom. By reason of them being set-back within the private plot and not being directly overlooked by any facing window, the windows are acceptable and resulting occupiers would not be subjected to any unacceptable overlooking impact.
- 8.41 The LPA issued pre-application advice 14.01.19 (18/03224/PREAPP) post the refusal of the 2no. previous applications affecting the site. Within the pre-application response letter, officers recognised the steep changes in land levels on site; which means development of an excessive scale, could appear overbearing. The pre-application proposal in terms of layout, reflects largely, that as proposed under this application. Officers outlined that the proposing of basement level accommodation largely resolves the overbearing nature of previous schemes on the site and thus; would not likely appear unacceptably overbearing to occupiers of no's 9 and 10 Alderwood Mews. However, officers advised robust information be presented to the LPA in the event of any planning application, to ensure a thorough understanding of any development's impacts.
- 8.42 Outside of matters covered in instances where a LPA deems it required for contractors to enter into a Considerate Contractors agreement, noise from the construction of a development is not a material consideration in the determination of a planning application. The intensification of the residential use on site; considering the distances between proposed dwellings and surrounding neighbouring existing/approved, as such that development is not understood to result in any unacceptable impact to neighbouring amenity by reason of noise.

- 8.43 By reason of the utilising of basement level accommodation; thereby reducing the scaling and massing of the development, the development is not considered unacceptably overbearing to neighbouring occupiers in spite of the topography of the site. Distances of proposed windows from boundaries and from neighbouring elevations demonstrate that development would not result in any unacceptable overlooking impact also. The submitted daylight and sunlight assessment, demonstrates that the development would not result in any unacceptable adverse impact to neighbouring residential amenity.
- 8.44 For reasons outlined, development complies with the objectives of the NPPF, (2019), policy 7.6 of the London Plan (2016) and Policy CP30 of the Enfield Core Strategy (2010).

Highways, Access and Refuse

- 8.45 Representations received which objected on the basis of development's impacts on highways matters/refuse implications, were on the following grounds:
- Inadequate access arrangement;
 - Increase in traffic;
 - Insufficient vehicular parking;
 - Refuse collection;
- 8.46 Policy 6.3 of the London Plan requires that the impact of development proposals on transport capacity and the transport network are fully assessed. The proposal must comply with policies 6.9 (cycling), 6.10 (walking), 6.11 (tackling congestion) and 6.13 (parking). Policies DMD 45 and 47 of the Enfield Development Management Document (2014) provide the criteria upon which developments will be assessed with regard to parking standards / layout and access / servicing.
- 8.47 London Plan Policy 6.13(e) suggests that within Outer London Boroughs, local policy should consider more generous standards (than those outlined within Table 6.2) in areas of low public transport accessibility (generally PTALs 0-1).

8.48 Policy DMD 45 seeks to minimise car parking and to promote sustainable transport options. The Council recognises that a flexible and balanced approach needs to be adopted to prevent excessive car parking provision although also, considering other factors affecting the acceptable quantum of parking delivered on a site:

- a. The scale and nature of the development
- b. The public transport accessibility (PTAL) of the site;
- c. Existing parking pressures in the locality;
- d. Accessibility to local amenities, and the needs of the future occupants of the developments.

Vehicular Parking

8.49 The site has a PTAL level of 1A which represents very poor accessibility to public transport. The development proposes 8no. long-stay vehicular parking spaces. All 8no. long-stay are to be integrated with charging technology for electric vehicles which exceeds the 20% recommended provision stipulated in the London Plan. Vehicular parking in its quantum is acceptable. The Local Highways Authority were consulted about the development and state no objection to the proposed level of car-parking noting the site's low PTAL Level.

8.50 Due to the small nature of the proposed development, development would not result in any unacceptable significant increase to vehicular traffic.

Access

8.51 Submitted Proposed Site Plan (Dwg. No. 479318-3) shows the proposed site layout. The site is accessed from the north side of Camlet way along an existing lane; between 39 and 41 Camlet Way.

8.52 The existing access measures 3.86 metres in its width and 32 metres in its length (measured from GIS). Whilst relevant guidance encourages two-way vehicle movement (which would not be possible along the lane), the Local Highways Authority state noting the low volume of traffic (both pedestrian and cars), the continued use of the access is acceptable. It should be noted the lane would also serve the 2no. existing vehicular garages outside of the red line of the site, in the ownership of occupiers of existing dwellings fronting Camlet Way also. Notwithstanding, the LPA concludes that noting the lane is existing, and the small scale of development proposed, the access arrangement is on balance acceptable.

8.53 The London Fire Brigade/Emergency Services were consulted about the application however have provided no comment. The applicant's Building Control Consultant stated that the below measures are to be integrated into the design of the development:

- a) A dry rising main box at the site entrance (adjacent public highway).
- b) A horizontal above ground 100 mm diam. Dry main pipe running to the refuse store unit.
- c) A dry main cabinet with twin outlet breach adjacent the refuse store. Main system must be drainable.
- d) All locations in all houses to be within 45 metres of dry main outlet OTHERWISE house or houses beyond same to be fitted with sprinklers to BS 9251 throughout.

8.54 The Council's Building Control Officers have stated that the above proposed would meet the requirements of the Building Regulations. For the purposes of this planning application, the LPA has no evidence to suggest that the access arrangements the safety of any end occupier; nonetheless, a separate building control application would require thorough assessment of the matter.

Cycle Parking

8.55 Submitted Proposed Site Plan outlines a shed located within each rear garden to include provision for cycle parking. Noting the spacious nature of the proposed development and that all units have private secure garden areas, this approach is on balance acceptable.

Refuse Provision

8.56 Submitted Proposed Site Plan outlines that all dwellings have independent refuse/recycling storage enclosures within the communal area of the site for the storage of bins.

8.57 The applicant outlines within the submitted Design & Access Statement that a private refuse collection company is provide all refuse services on behalf of the occupants rather than any Council collection being utilised. The applicant has submitted a swept path analysis plan (dwg. no. 10955-001) to demonstrate that a refuse collection vehicle (measured at a length of 6.623 metres) can turn with the site and enter/egress the site in a forward gear. The information is sufficient at

demonstrating that a small/medium refuse vehicle would be capable of manoeuvring within the site.

- 8.58 The principle of a private refuse/servicing management arrangement is only acceptable in circumstances where it is demonstrated; for site specific circumstances, why this represents the most appropriate solution. The approach is somewhat discouraged as it is difficult for the LPA to enforce upon non-compliance with a private arrangement. Notwithstanding, in this instance, noting the limitations of the access arrangement, and that the LPA does not wish to promote a significant number of bins on the junction; for reasons of both visual amenity but also highways safety, the private arrangement is concluded appropriate in this instance.

Trees and Landscaping

- 8.59 The applicant has submitted an Arboricultural Impact Assessment (dated 05.10.17 and updated 10.04.19). The document outlines the development proposal would require the removal of eleven trees (pg. 12 of document outlines tree numbers). The submitted document also includes a Tree Protection Plan which outlines root protection areas, and temporary protection measures are to be integrated.
- 8.60 The site neither lies within any conservation area nor is the site affected by any trees with Tree Preservation Orders (TPOs). The Borough Tree Officer was not consulted on the application. Notwithstanding, there was no objection by the LPA to the previous version of the document submitted with application 17/04406/FUL. The tree protection plan would be appropriately conditioned should the application be supported and is acceptable in compliance with Policy DMD80 within the Development Management Document (2014).

Sustainable Drainage

- 8.61 London Plan policies 5.12 and 5.13 require the consideration of the effects of development on flood risk and sustainable drainage respectively. Core Policy 28 (Managing flood risk through development) confirms the Council's approach to flood risk, inclusive of the requirement for SuDS in all developments Policy DMD 61 (Managing Surface Water) expects a Drainage Strategy will be required for all developments to demonstrate how proposed measures manage surface water as close to its source as possible and follow the drainage hierarchy in the London Plan. All developments must maximise the use of and, where possible, retrofit Sustainable Drainage Systems (SuDS) which meet policy requirements.

- 8.62 The applicant has submitted a Sustainable Drainage Strategy (Ref. 19064/SUDs_R01/RS Revision P1 dated 15/05/19) with the application. The document outlines the approach to integrating mitigation measures to aid drainage of the site. The document outlines that the development results in a reduction to the amount of the site covered with impermeable surfacing. As existing, 900sqm of the site is covered by impermeable hard surfacing, this is reduced to 670sqm as a result of the proposed development. The applicant also proposed 270sqm of green roofs across the site. All first floor level flat roof areas are to be green sedum roofs. Should the development be granted planning permission, further detail about the green roofs would be required. The above measures are compliant with Policy DMD59; *Avoiding and Reducing Flood Risk* of the Development Management Document (2014).
- 8.63 Should the application be granted planning permission, a condition would be placed on the decision, requiring prior to the commencement of development, the applicant submit to the LPA and have approved, a ground water flood risk assessment to ensure the basement accommodation is safe from flooding and development does not impede ground water flows.

Ecology

- 8.64 *Bat Emergence and Re-Entry Surveys* document (prepared by ARBTECH Consultants, final draft dated 07.08.19) accompanies the application. The methodology for assessing the existing dwelling's scope for bat roosting was assessed by a desk study and a field study including both an internal and external survey. Bats were heard however no emergence or re-entry activity was recorded at times tested. The report concludes whilst it is unlikely there is current roosting within the existing building, that appropriate bat boxes be designed into the all of the proposed buildings. The submitted information is adequate and the mitigation measure suggested, concluded appropriate.
- 8.65 A compliance type condition would be applied to the decision notice should planning permission be granted to ensure the applicant integrate ecological enhancement measures into the redevelopment of the site inclusive of the integration of bird and bat boxes in compliance with Policy DMD79; *Ecological Enhancements*, of the Development Management Document (2014).

Energy

- 8.66 Policy 5.2 of the London Plan (2016) expects development proposals to make the fullest contribution to minimising carbon dioxide emission and Enfield Core Strategy Policy CP4 sets a strategic objective to achieve the highest standard of sustainable design and construction throughout the Borough. Policy DMD 50 (Environmental Assessment Methods) required the proposed Development to achieve Code Level 4 (or equivalent rating if this scheme is updated) where it is technically feasible and economically viable to do so. The adopted policies require that new developments achieve the highest sustainable design and construction standards having regard to technical feasibility and economic viability. A 35% CO2 reduction over Part L of Building Regulations (2013) is required.
- 8.67 The applicant has submitted an Energy Statement (prepared by Energy Test Ltd. Dated 23.04.19) which outlines the development will exceed Part L of Building Regulations (2014) and achieve a 35% CO2 reduction. Should the development be granted planning permission, the LPA would require a condition which shows at the stage of practical completion, this reduction has been at least achieved or exceeded.

Water Consumption

- 8.68 Policy DMD 58 (water Efficiency) expects new residential development, including new build and conversions, will be required to achieve as a minimum water use of under 105 litres per person per day.
- 8.69 The applicant has submitted an Energy Statement (prepared by Energy Test Ltd. Dated 23.04.19) which outlines that the 105 litre per person per day level will not be exceed. This is acceptable and should the development be granted planning permission, a compliance condition would be recommended to ensure the development does not exceed the level outlined.

9. Section 106 Agreement

Affordable housing contribution

- 9.1 Chapter 5 (Delivering a Sufficient Supply of Homes) of the updated NPPF (2019) expects residential developments to provide a size, type and tenure of housing needed for different groups in the community, forming a core element of housing provision reflected in planning policies.

- 9.2 Policy 3.13 (Affordable Housing Thresholds) of the adopted London Plan (2016) states Boroughs are encouraged to seek a lower threshold through the LDF process where this can be justified in accordance with guidance, including circumstances where this will enable proposals for larger dwellings in terms of floorspace to make an equitable contribution to affordable housing provision.
- 9.3 Following the Court of Appeal decision on 11 May 2016, policies CP3 of the Core Strategy and Policy DMD2 of the Development Management Document are now defunct and do not sit within the scope of the National Policy exemptions. As per the London plan policy 3.13 and guidance in the DMPO (2015) which has yet to be formally revised the development site is considered to represent a minor residential development. Noting this, the LPA would not seek to secure financial obligations in the form of a S106 agreement from the applicant.

10. CIL Financial Contribution Payable

- 10.1 The development shall pay the following CIL contributions upon commencement of development.

Mayoral CIL

- 10.2 The Mayoral CIL is collected by the Council on behalf of the Mayor of London. The amount that is sought is for the scheme is calculated on the net increase of gross internal floor area multiplied by the Outer London weight of £60 together with a monthly indexation figure. It is noted as of the 1st of April 2019 Mayoral CIL has increased to £60/m².

- 10.3 Mayoral community infrastructure levy (CIL) is payable, based on the submitted CIL Form, on the basis of 536sqm of additional gross floor area, which from 1 April 2019 will be calculated at £60 per sqm:

$$536 \times £60 = £ 32, 160. 00$$

Enfield CIL

- 10.4 On 1 April 2016, the Council introduced its own CIL. The money collected from the levy (Regulation 123 Infrastructure List) will fund rail and causeway infrastructure for Meridian Water.

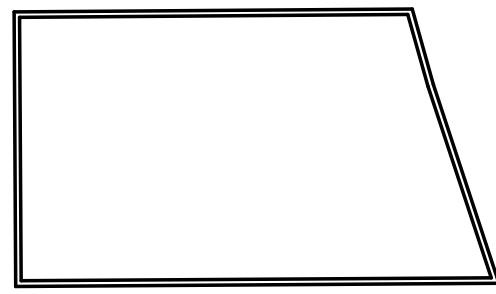
- 10.5 The Council CIL payment should therefore be as follows based on the estimated net additional gross floorspace in the submitted CIL form:

$$536 \quad \times 120 = \text{£ } 64, 320. 00$$

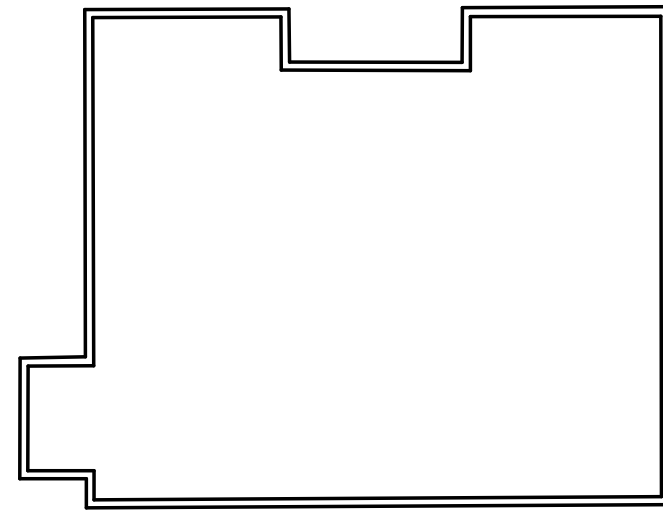
11. Conclusion

- 11.1 The LPA concludes the applicant has adequately overcome the reasons for refusal attached to previously refused applications affecting the site and the proposed development is acceptable for reasons outlined within this report; subject to appropriate conditions. The application largely reflects the submitted pre-application scheme and the applicant has front loaded technical detail which results in a robust submission.

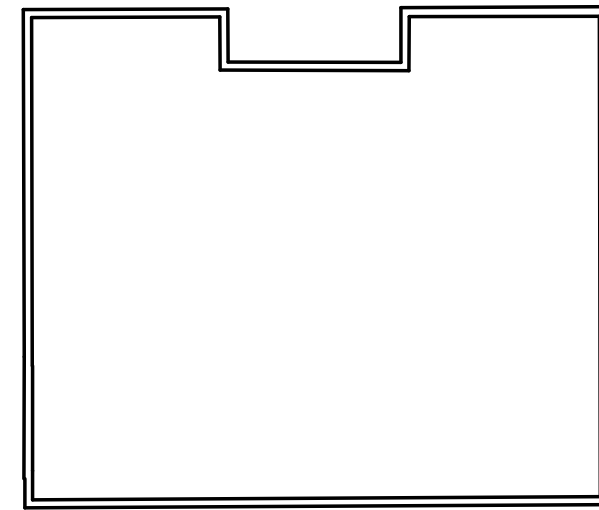
TOTAL GIFA FOR ALL BUILDINGS ON SITE- 446.8M2



EXISTING GARGE (1:200) 79M2



EXISTING GF (1:200) 183.9M2



EXISTING LGF (1:200) 183.9M2

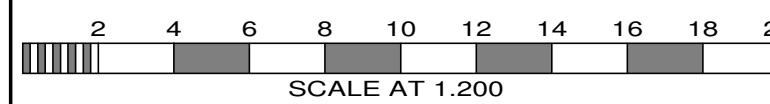
PLANNING



EXISTING LOCATION PLAN (1:1250)

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SITE AREA 2204M2



No.	Date	By	Comments
Revisions			

Site Address
**39A CAMLET WAY
 BARNET
 EN40LJ**

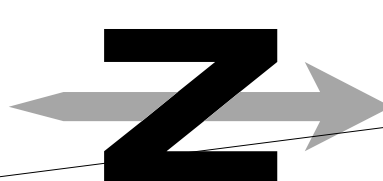
Job Title
**DEMOLITION OF EXG
 HOUSE AND GARAGE
 AND ERECTION OF 4 HOUSES**

Drawing Title
EXISTING SITE PLAN

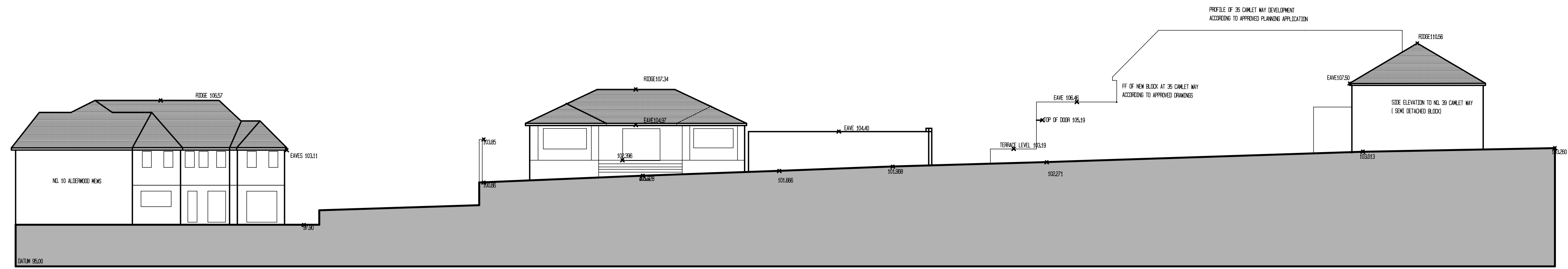
Scale 1:200 @ A | Date 05/18 | Drawn by AJC

Alan Cox
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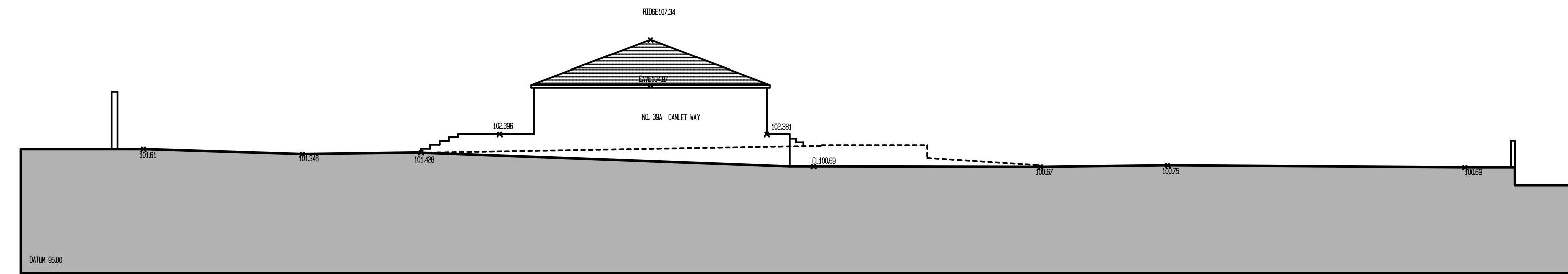
Org.No. 479318-1 | Rev.



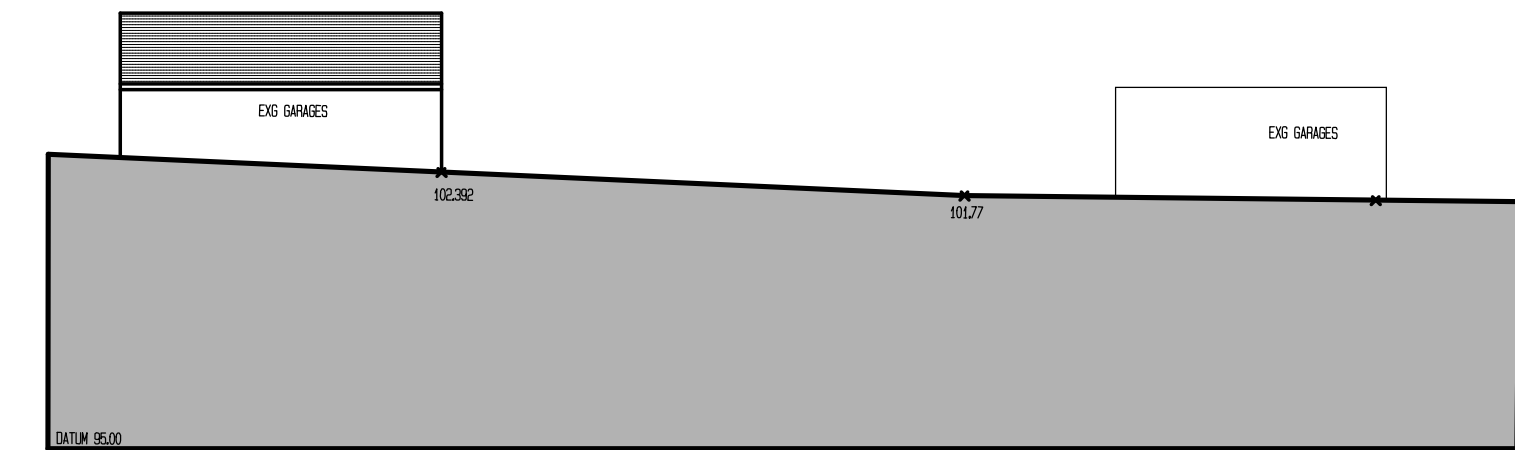
EXISTING SITE PLAN (1:200)
 EXISTING HARD STANDING ON SITE INCLUDING EXG BUILDINGS/ ACCESS ROAD - 1050M2



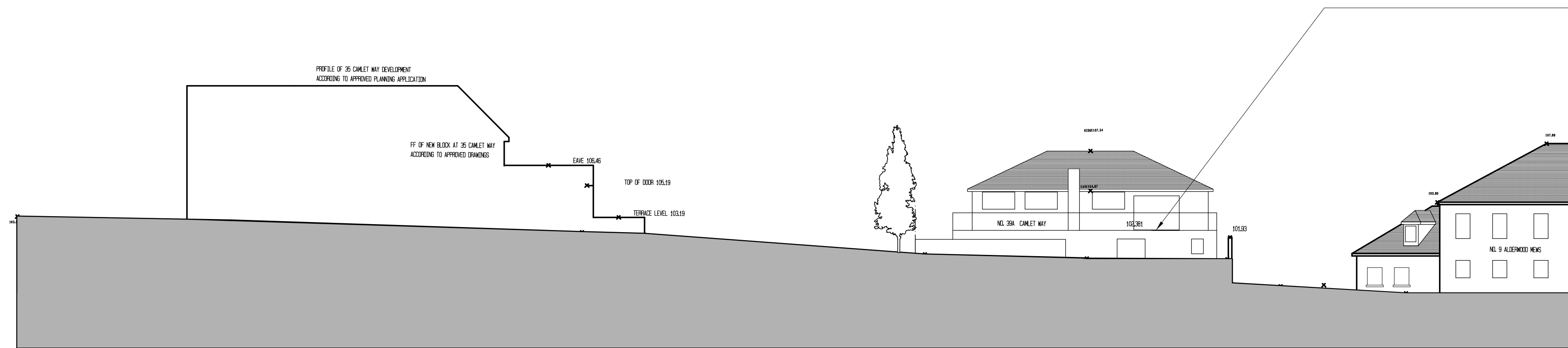
SECTION AA



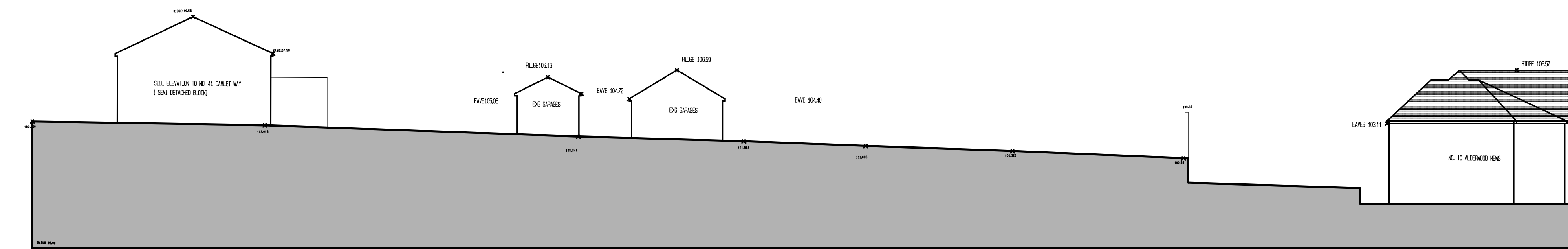
SECTION BB



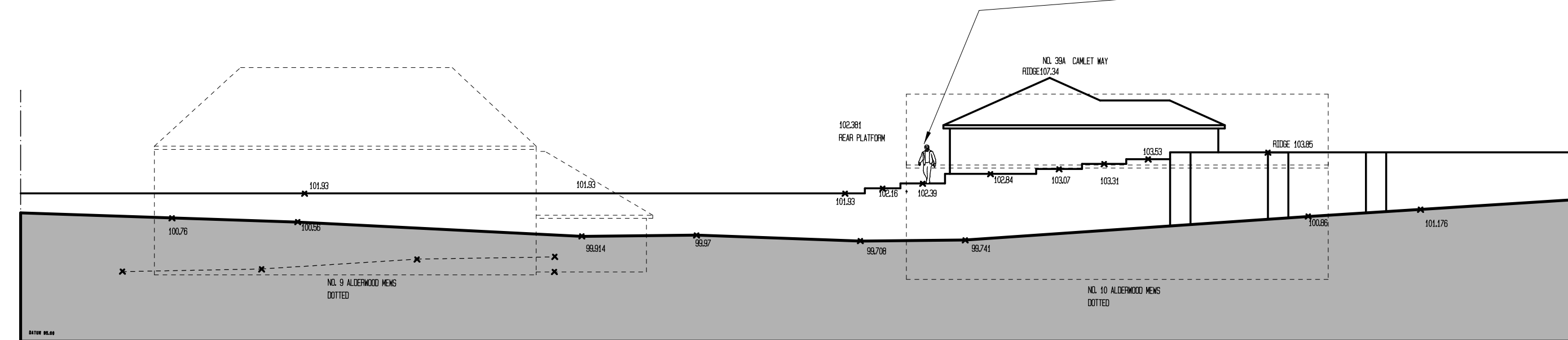
SECTION CC



SECTION DD



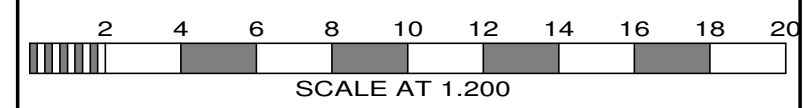
SECTION EE



SECTION FF



EXISTING OVERLOOKING FROM THE RAISED PLATFORM OF 39A CAMLET WAY



No.	Date	By	Contents

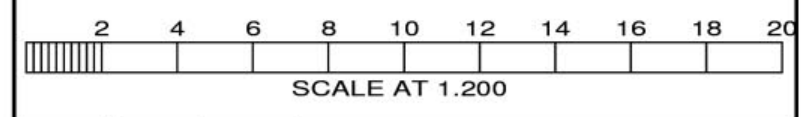
Revisions			
Site Address			
39A CAMLET WAY BARNET EN40LJ			

Job Title
DEMOLITION OF EXG HOUSE AND GARAGE AND ERECTION OF 4 HOUSES

Drawing Title
EXISTING SITE SECTIONS

Scale 1:200 @ A1	Date 05/18	Drawn By AJC
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Org.No. 479318-2	Rev.
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No.	Date	By	Contents
Revisions			

Site Address
**39A CAMLET WAY
 BARNET
 EN40LJ**

Job Title
**SINGLE STOREY REAR
 EXTENSION AND INTERNAL
 ALTERATIONS**

Drawing Title
PROPOSED SITE PLAN

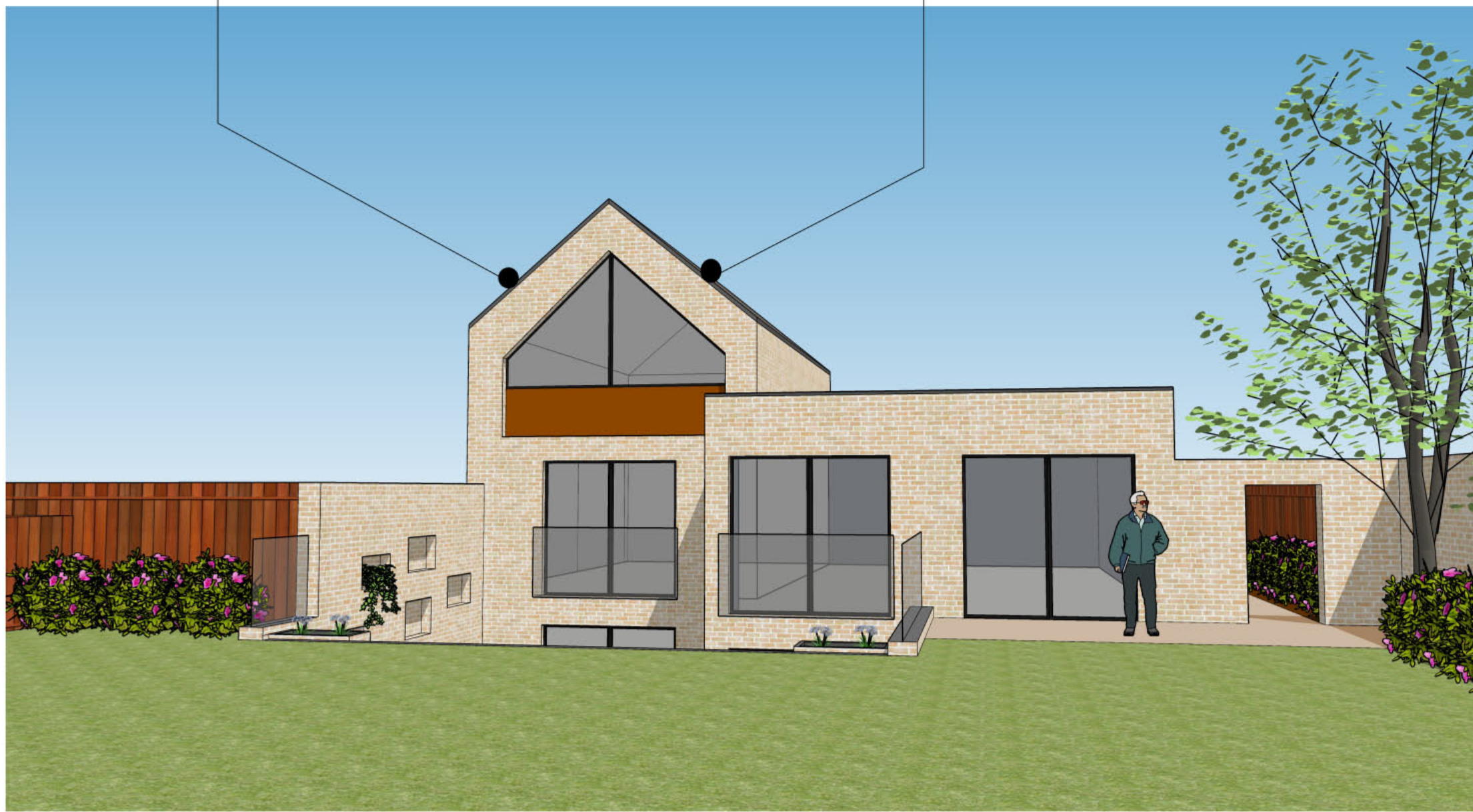
Scale **1.200 @ A1** Date **05/18** Drawn By **AJC**

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 associates**
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Proj. No. **479318-3** Rev.

PITCHED ROOFS REFERENCES COTTAGES AT FRONT OF SITE AND EXG GARAGES WITHIN SITE

GREY SLATE TILES



PLOT 1 CGI

LIGHTWELLS



PLOT 2 CGI

EXG WALL ALONG BOUNDARY TO ALDERWOOD MEWS TO BE RETAINED

LANDSCAPING BY OTHERS

PERMEABLE SHINGLE



PLOT 3 CGI

CORTEN PANNELLING



GREEN SEDUM ROOFS TO ALL FLAT ROOFS

CREEPER PLANTS IN BRICK WALL

STEPS UP TO GARDEN

PLANTERS BUILT INTO STEPS

CGI SHOWING LIGHTWEL (TYPICAL)



No.	Date	By	Contents
Revisions			

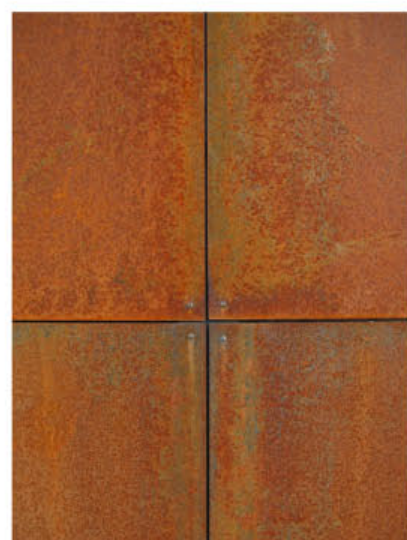
Site Address
**39A CAMLET WAY
 BARNET
 EN40LJ**

Job Title
DEMOLITION OF EXG HOUSE AND GARAGE AND ERECTION OF 4 HOUSES

Drawing Title
INDICATIVE CGI WITH MATERIAL PROPOSAL

Scale: NTS Date: 04/19 Drawn By: AJC

MATERIAL PALLET



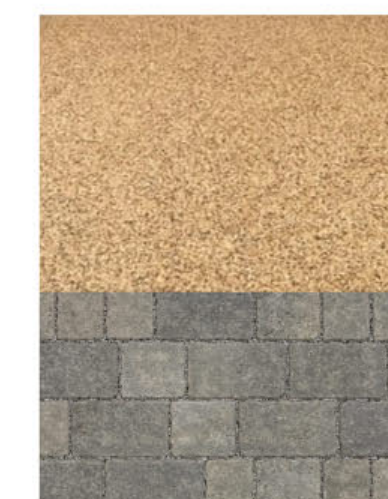
CORTEN PANELS REPRESENT THE NATURAL FEEL OF THE SITE. THE EARTH COLOUR OF THE STEEL ENHANCES THE BROWN WOOD OF THE TREE TRUNKS WITHIN THE LANDSCAPING



MOTTLED BAIGE LIGHT BRICKS COMPLIMENT THE SCHEME AT NO. 35 CAMLET WAY AND CONTRAST WITH THE NEUTRAL COLOURS OF THE CORTEN, WINDOWS AND ROOF TILES



GREY SLATE ROOF TILES REFLECT THE HOUSES FRONTING CAMLET WAY



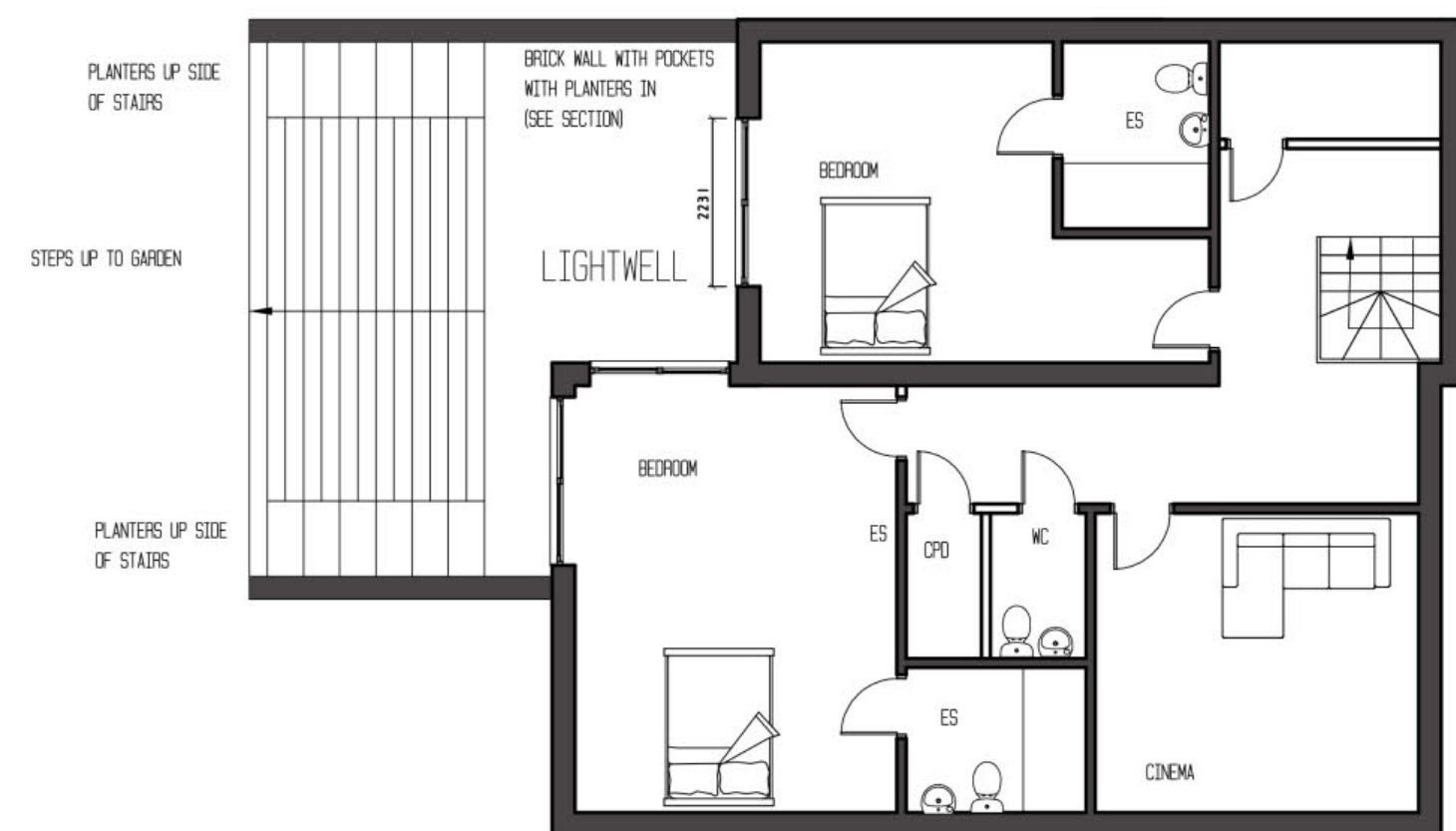
PERMEABLE SHINGLE TO NEW ACCESS AREA AND PERMEABLE PAVING TO DRIVEWAY TO BE CONFIRMED BY WAY OF CONDITION WITH SUDS SCHEME BY SPECIALIST

Alan Cox
 associates
 Architectural & Planning Consultants
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 FAX: 020 - 8364 - 9556

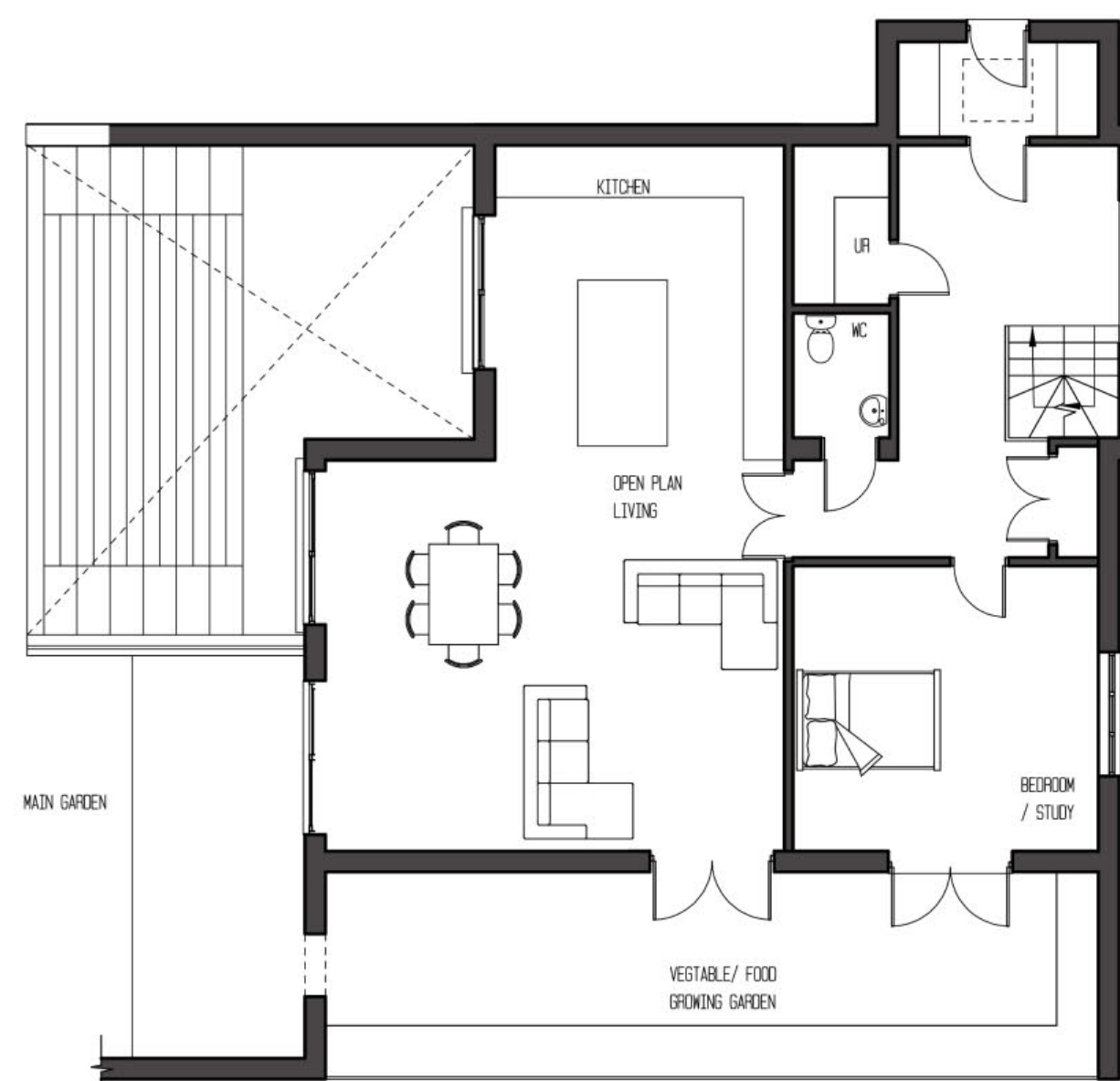
Org.No. 479318-4 Rev.

PLANNING

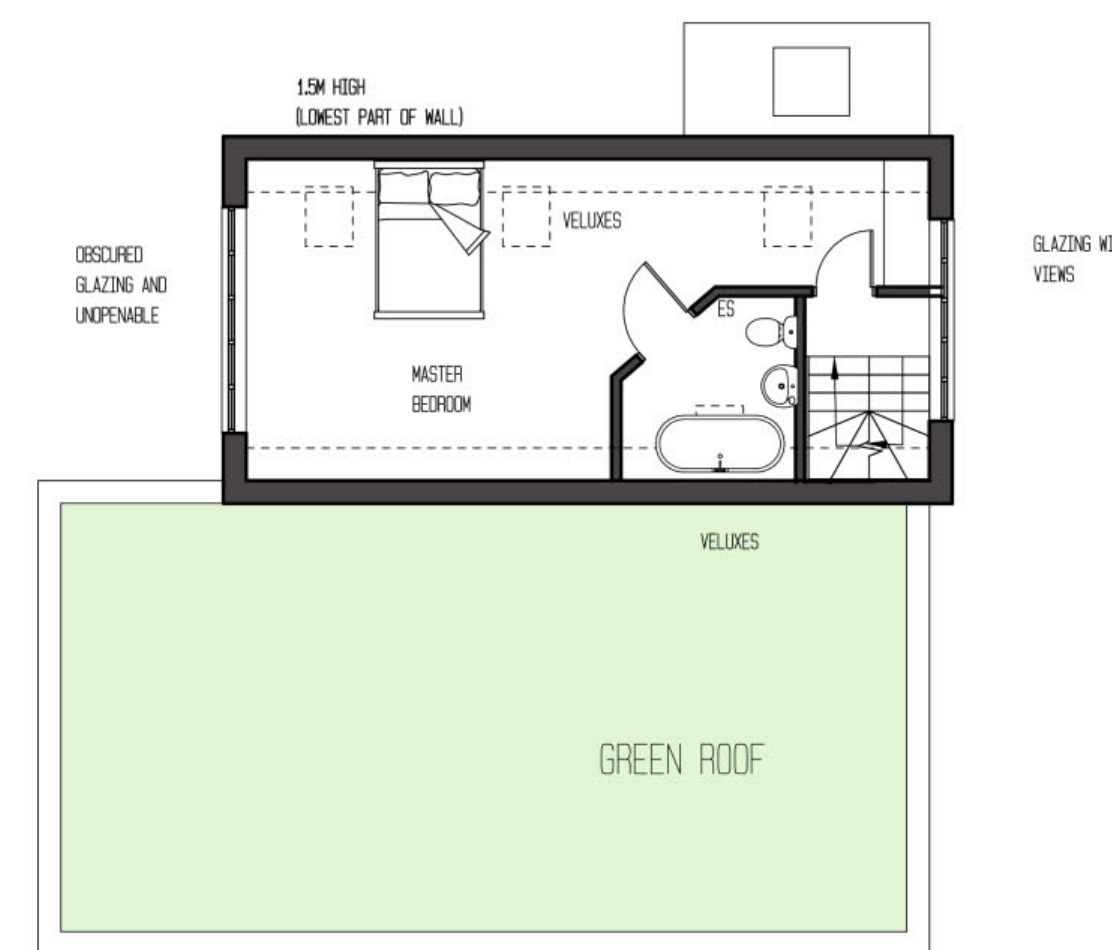
GIFA (251M2)



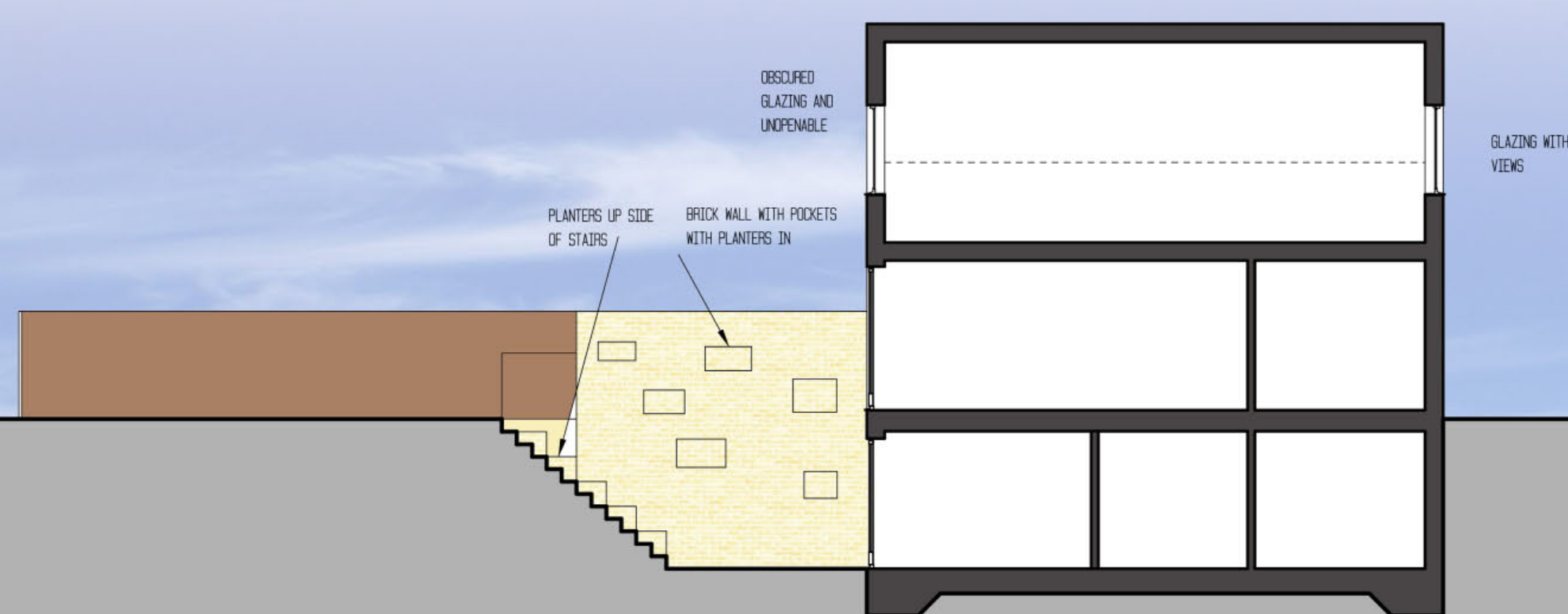
BASEMENT PLAN (104M2)



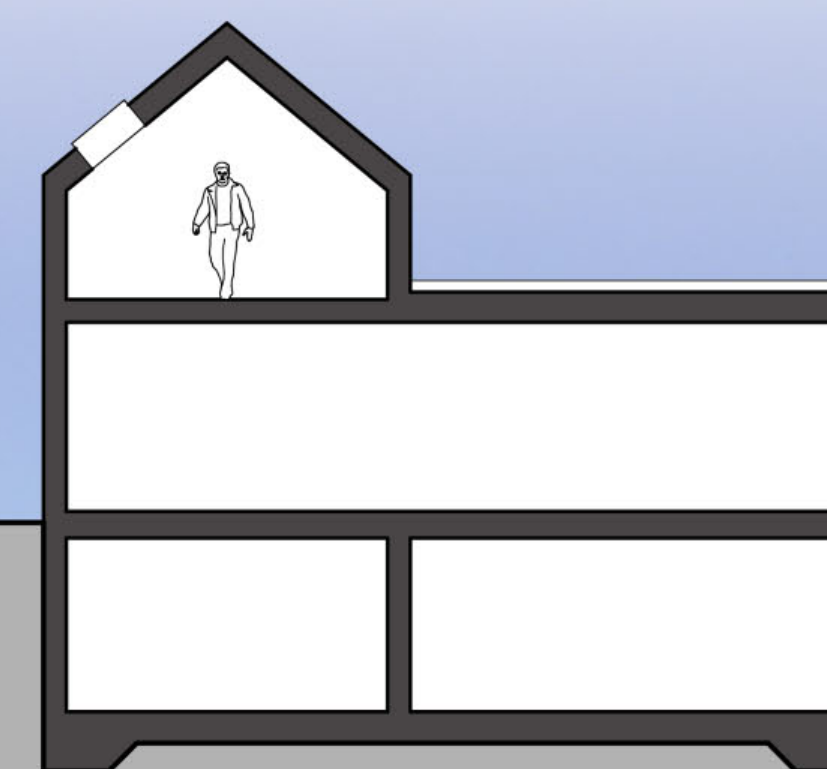
GROUND FLOOR PLAN (109M2)



FIRST FLOOR PLAN (38m2)



LONG SECTION



SHORT SECTION



SIDE ELEVATION (FACING GARDEN)

REAR ELEVATION (FACING FOOD GARDEN)

SIDE ELEVATION (FACING INTO SITE)

FRONT ELEVATION (FACING DRIVEWAY)

No.	Date	By	Contents
Revisions			

Site Address
**39A CAMLET WAY
 BARNET
 EN40LJ**

Job Title
**DEMOLITION OF EXG
 HOUSE AND GARAGE
 AND ERECTION OF 4 HOUSES**

Drawing Title
**PROPOSED DETAILS
 PLOT 1**
 Scale 1:100 @ A1 Date 04/19 Drawn by AJC

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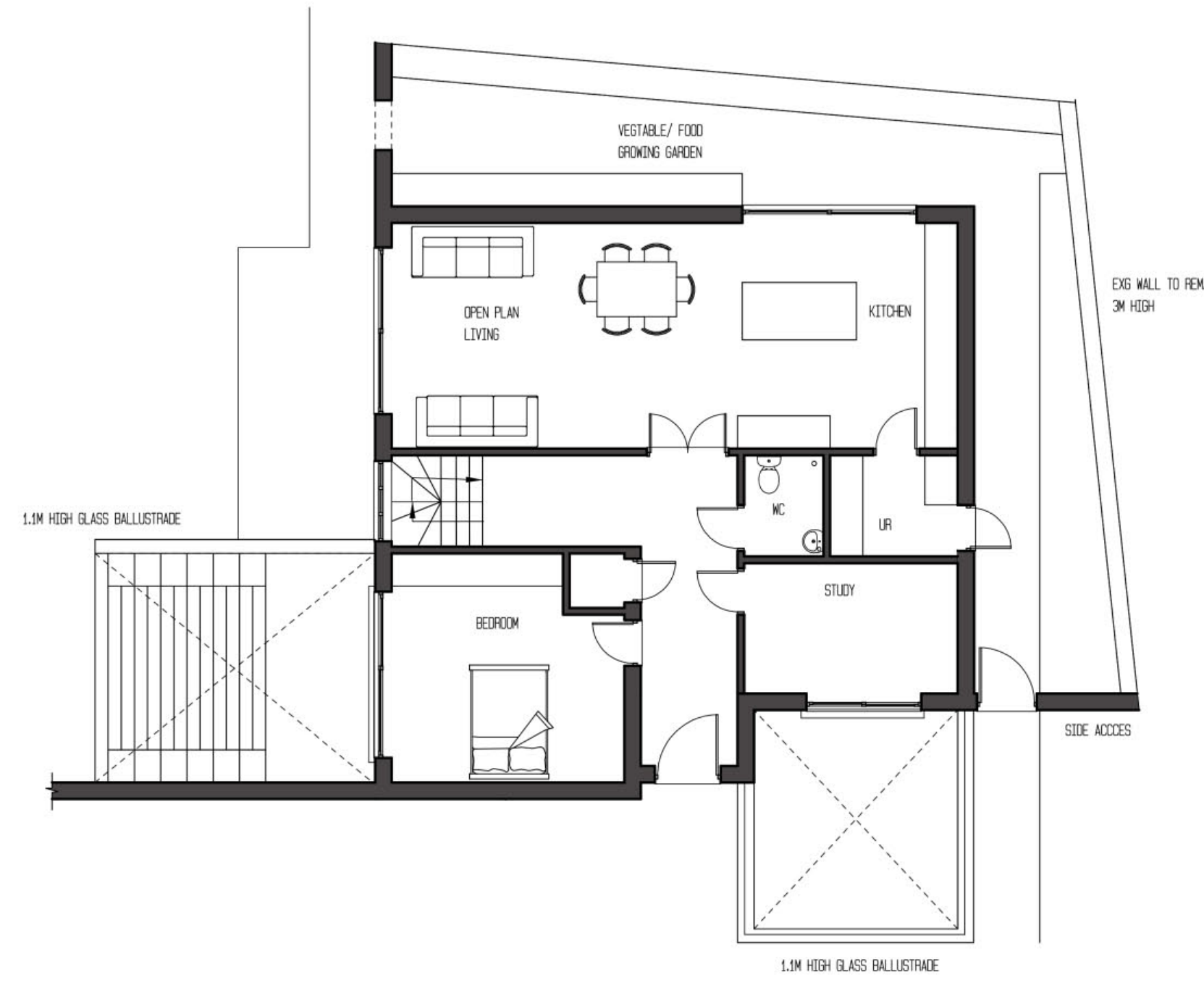
Org.No. 479318-5 Box

PLANNING

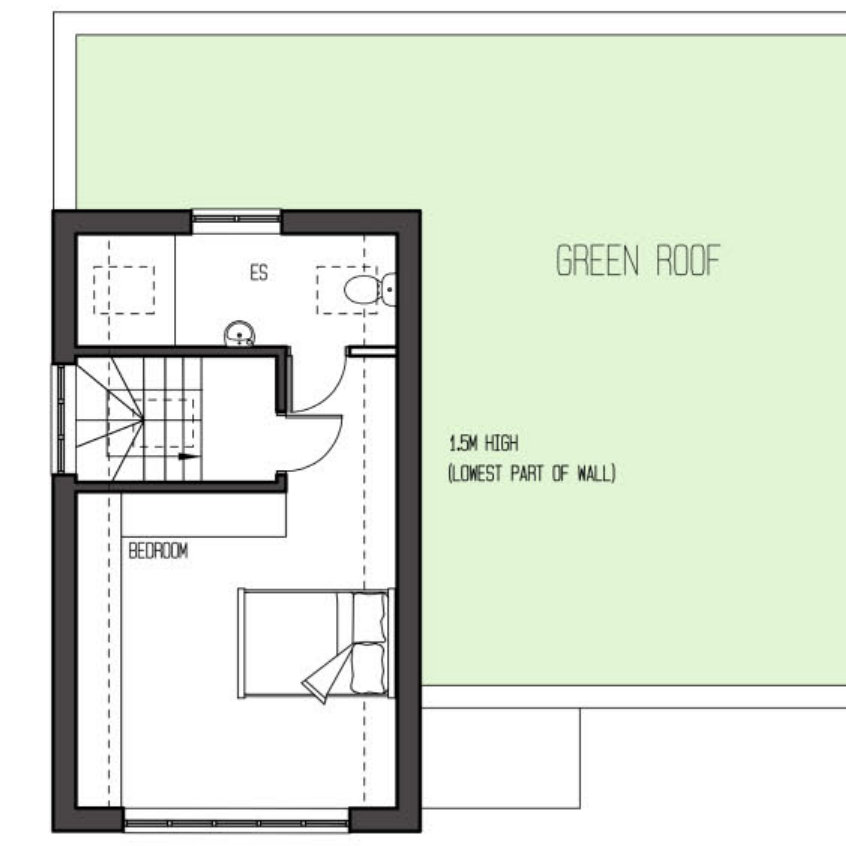
GIFA (229M2)



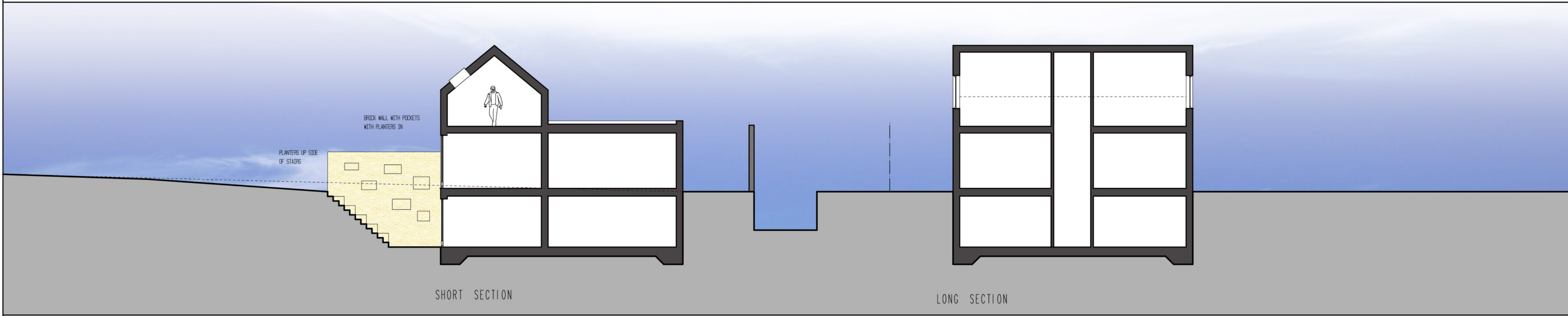
BASEMENT PLAN (98M2)



GROUND FLOOR PLAN (98M2)



FIRST FLOOR PLAN (32M2)



SHORT SECTION

LONG SECTION



SIDE ELEVATION (FACING GARDEN)

REAR ELEVATION (FACING FOOD GARDEN)

FRONT ELEVATION (FACING DRIVEWAY)

SIDE ELEVATION (FACING BOUNDARY)

No.	Date	By	Contents
Revisions			

Site Address
**39A CAMLET WAY
 BARNET
 EN40LJ**

Job Title
**DEMOLITION OF EXG
 HOUSE AND GARAGE
 AND ERECTION OF 4 HOUSES**

Drawing Title
**PROPOSED DETAILS
 PLOT 2**

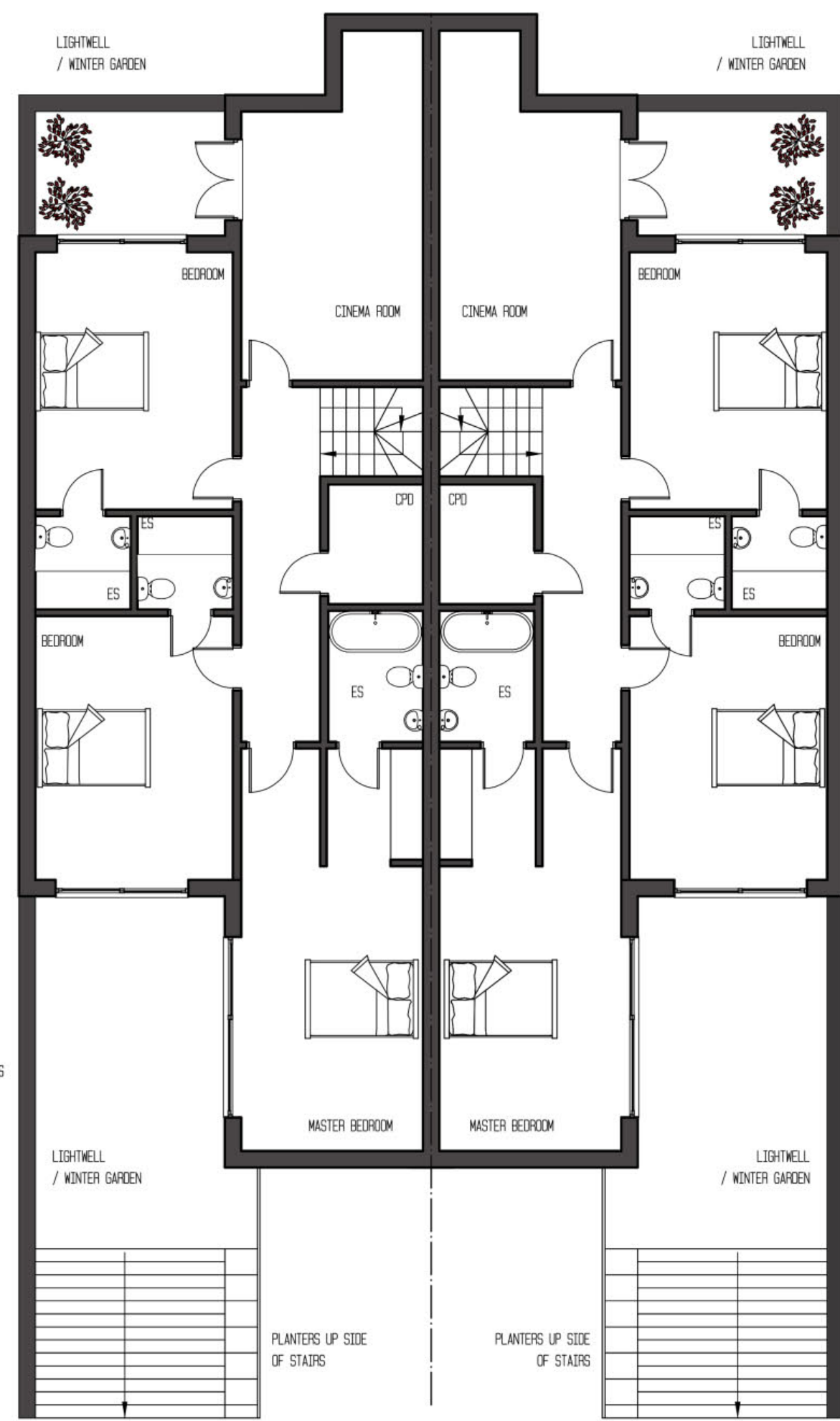
Scale 1:100 @ A1 Date 04/19 Drawn by AJC

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 Architectural & Planning Consultants
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 FAX: 020 - 8364 - 9556

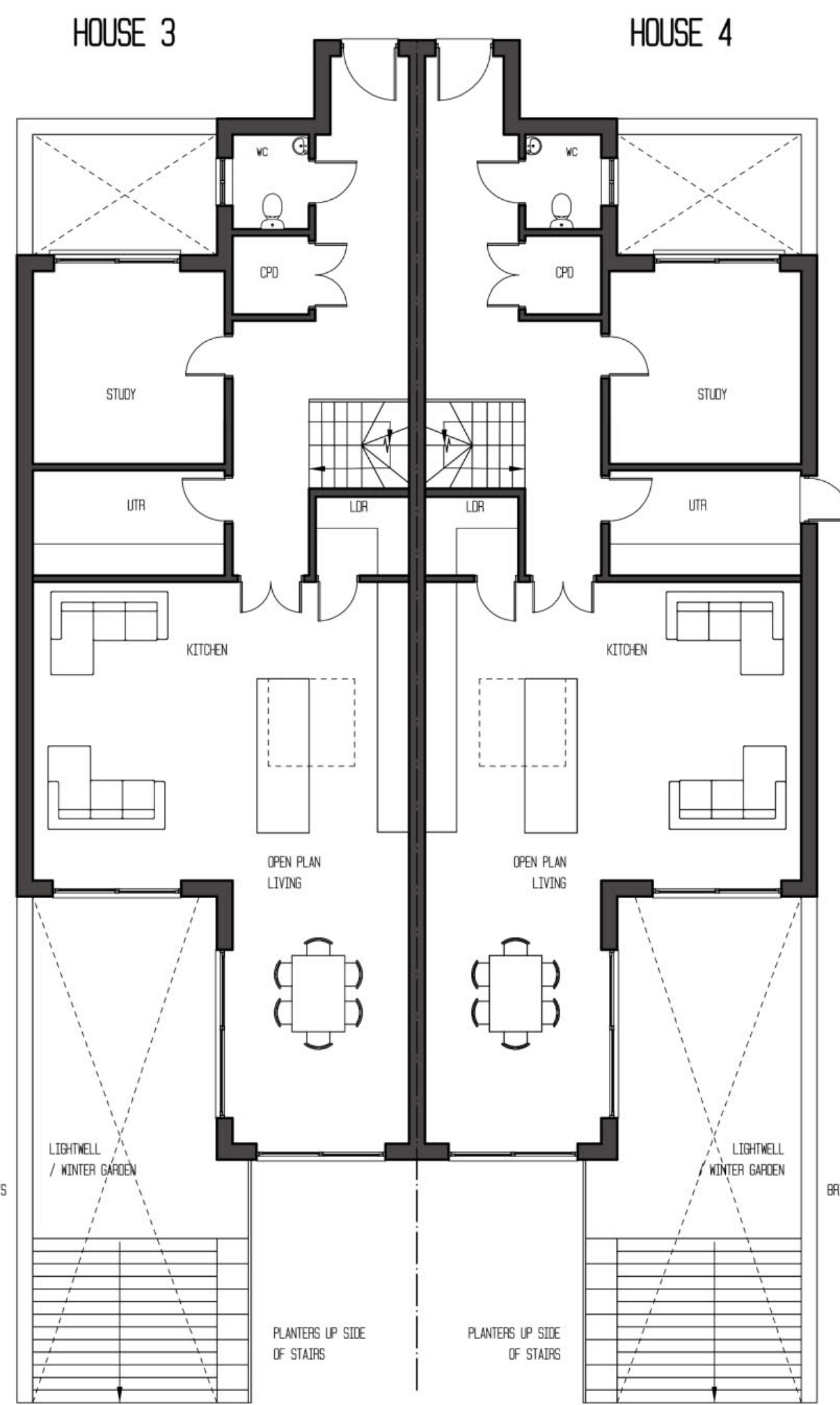
Org.No. 479318-6 Box

PLANNING

GIFA (251M2 PER HOUSE)



BASEMENT PLAN (111M2 PER HOUSE)



GROUND FLOOR PLAN (111M2 PER HOUSE)



BASEMENT PLAN (29M2 PER HOUSE)



REAR ELEVATION (FACING GARDEN)



REAR ELEVATION (FACING ALDERWOOD MEWS BOUNDARY)



REAR ELEVATION (FACING 35 CAMLET WAY)



REAR ELEVATION (FACING GARDEN)

No.	Date	By	Contents
Revisions			

Site Address
**39A CAMLET WAY
 BARNET
 EN40LJ**

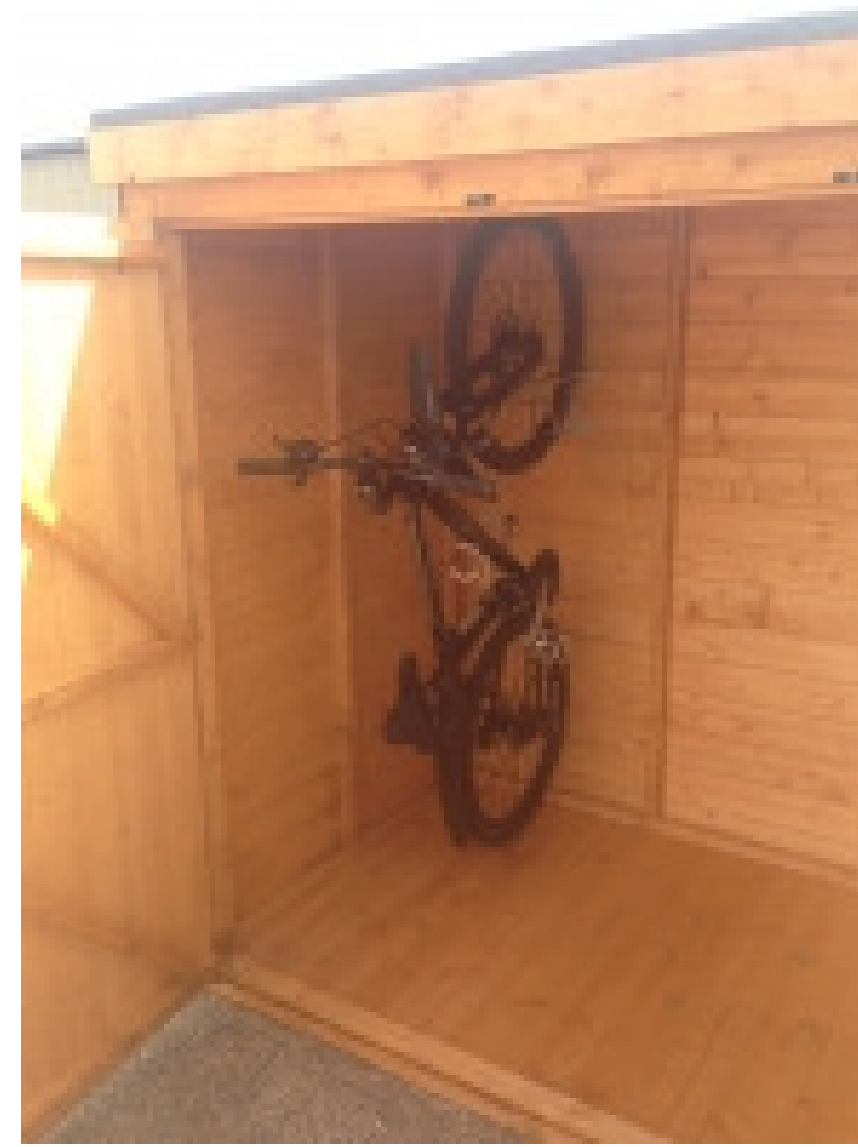
Job Title
**DEMOLITION OF EXG
 HOUSE AND GARAGE
 AND ERECTION OF 4 HOUSES**

Drawing Title
**PROPOSED DETAILS
 PLOT 3/4**

Scale 1:100 @ A1 Date 04/19 Drawn by AJC

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Org.No. 479318-7 Box



SECURED BY DESIGN HANGER SHED

LARGE SHED WITH ROOM FOR 4 BICYCLES

REFUSE AND RECYCLING

EACH HOUSE HAS A DESIGNATED AREA FOR REFUSE/ RECYCLING/ FOOD WASTE / GARDEN WASTE
 EACH SPACE ALLOWS FOR 240 LITRE BINS (4N0) AND WILL BE COLLECTED BY A PRIVATE REFUSE VEHICLE



No.	Date	By	Comments
Revisions			

Site Address
 39A CAMLET WAY
 BARNET
 EN40LJ

Job Title
 DEMOLITION OF EXG
 HOUSE AND GARAGE
 AND ERECTION OF 4 HOUSES

Drawing Title
 REFUSE/RECYCLING DETAILS
 BIKE STORAGE

Scale NTS **Date** 04/19 **Drawn by** AJC

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associates
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Org.No. 479318-9 **Rev.**

LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 15th October 2019

Report of

Head of Planning

Contact Officer:

Andy Higham
Jennie Rebaire
0208-379-3822

Ward:

Southgate

Ref: 19/02435/HOU

Category: Householder

LOCATION: 47 Eversley Park Road, London, N21 1JJ

PROPOSAL: Construction of hard standing in connection with vehicular access and installation of electrical car charging unit.

Applicant Name & Address:

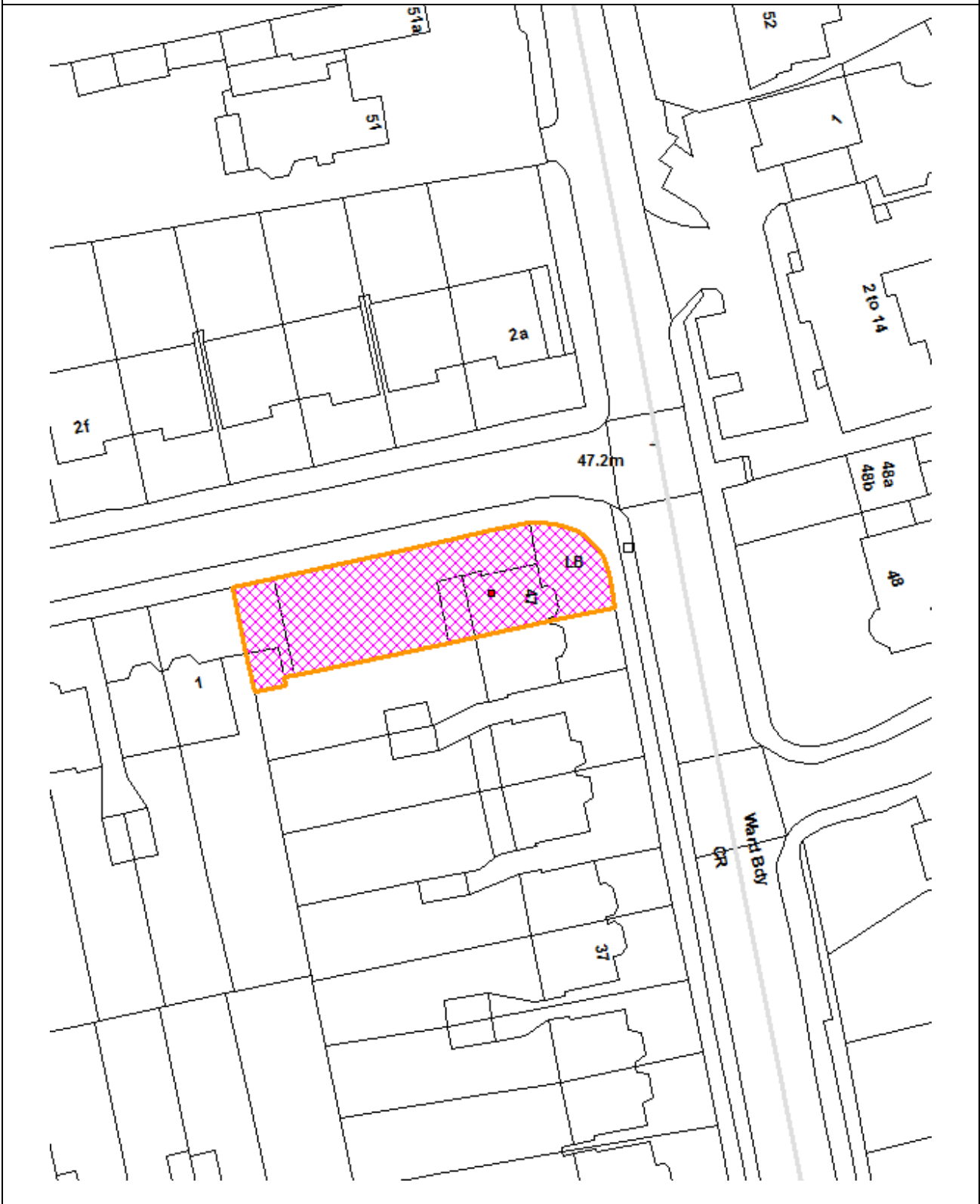
C/O the Agent

Agent Name & Address:

Mr Murat Aydemir
47 Eversley Park Road
London
N21 1JJ

RECOMMENDATION: That planning permission be REFUSED

Ref: 19/02435/HOU LOCATION: 47 Eversley Park Road, London, N21 1JJ,



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Scale 1:1250

North



1.0 Note for Members

- 1.1 Although a planning application for this type of development would normally be determined by officers under delegated authority, the application is reported to the Planning Committee for determination at the request of Councillor Ioannou.

2.0 Recommendation

That planning permission be REFUSED for the following reasons:

1. The proposed formation of vehicular accesses where there is an existing alternative access to off-street parking from Holly Hill would lead to vehicles stopping, slowing down and turning from or onto the highway close to the junction of Holly Hill and Eversley Park Road. Existing street furniture would further serve to reduce sightlines and increase the likelihood of vehicle movement from the site being detrimental to highway safety at this junction. In these respects, the proposal would be contrary to the Council's Revised Technical Footway Guidance 2013, Policy DMD 46 of the Development Management Document, and the Core Strategy policies 25 and 30, as well as the aims of the NPPF.
2. The loss of established hedge and associated soft landscaping to be replaced predominantly with hardstanding to facilitate off street car parking would cause harm to the character and appearance of the street scene and surrounding area by means of the loss of established hedge and soft landscaping which make a positive contribution to the street scene, contrary to Local Plan Policies CP30 and Development Management Document Policies DMD37, DMD46.

3.0 Site and Surroundings

- 3.1 The application property consists of a 2-storey semi-detached dwelling located on the corner of Eversley Park Road and Holly Hill within a predominantly residential area.
- 3.2 A vehicular access off Holly Hill and detached garage exist in the rear garden of the property.
- 3.3 The property fronts Eversley Park Road on the corner of its junction with Holly Hill. PTAL is 1 (poor). Eversley Park Road is a Classified Road and also a Bus Route with a significant level of traffic flow.
- 3.4 A number of front forecourts that are being used for the parking of vehicles are either historic or did not obtain the necessary permissions.

4.0 Proposal

- 4.1 Planning permission is sought for the vehicular crossover to Eversley Park Road and construction of hardstanding in the front garden area together with a secondary crossover from Holly Hill and the installation of electrical car charging unit.

5.0 Relevant Planning History

- 5.1 18/03035/HOU - Vehicular access with hard standing and installation of electric car charging unit was refused planning permission in September 2019 for the following reason:

- The formation of a vehicular access where there is an existing alternative access to off-street parking from Holly Hill would lead to vehicles stopping, slowing down and turning from or onto the highway close to the junction of Holly Hill and Eversley Park Road. Existing street furniture would serve to reduce sightlines and increase the likelihood of vehicle movement from the site being detrimental to highway safety at this junction. In these respects, the proposal would be contrary to the Council's Revised Technical Footway Guidance 2013, Policy DMD 46 of the Development Management Document, and the Core Strategy policies 24 and 30, as well as the aims of the NPPF.

- 5.2 An appeal (APP/Q5300/W/18/3218789) against this decision was dismissed with the Planning Inspector concluding that the proposal would be detrimental to highway safety. Therefore, it would not comply with Policy DMD 46 of the Development Management Document (adopted November 2014), Policies 24 and 30 of The Enfield Plan Core Strategy 2010 - 2025 (adopted November 2010) and the Revised Technical Standards for Footway Crossovers (April 2013) which, amongst other things, seek to safeguard highway safety for all users and ensure traffic flow is maintained.

- 5.3 17/03558/FUL - Vehicular access with hard standing was refused planning permission in October 2017 for the following reason:

- The formation of a vehicular access where there is an existing alternative access to off-street parking from Holly Hill would lead to vehicles stopping, slowing down and turning from or onto the highway close to the junction with Holly Hill and existing street furniture, thus adversely affecting the safety and free flow of traffic on a classified road and would constitute a hazard to pedestrian and highway safety. In these respects, the proposal would be contrary to the Council's Revised Technical Footway Guidance 2013, Policy DMD 46 of the Development Management Document, and the Core Strategy policies 24 and 30, as well as the aims of the NPPF.

- 5.4 An appeal (APP/Q5300/D/18/3193446) against this decision was dismissed with the Planning Inspector concluding that the proposal would be detrimental to highway and pedestrian safety contrary to Policy DMD 46
- 5.5 17/02437/HOU - Single storey front extension and front porch – Granted with Conditions July 2017
- 5.6 17/01500/HOU - Vehicular access with block paving and garden wall to front and single storey front porch extension – Refused May 2017
- The formation of a vehicular access where there is an existing alternative access to off-street parking from Holly Hill would lead to vehicles stopping, slowing down and turning from or onto the highway, thus adversely affecting the safety and free flow of traffic on a classified road and would constitute a hazard to pedestrian and highway safety. In these respects, the proposal would be contrary to the Council's Revised Technical Footway Guidance 2013, Policy DMD 46 of the Development Management Document, and the Core Strategy policies 24 and 30, as well as the aims of the NPPF.
 - The loss of an existing mature hedge and associated soft landscaping would result in visual harm to the detriment of the street scene in Eversley Park Road contrary to Policies 25 and 30 of the Core Strategy and Policies DMD8, 37 and 46 of the Adopted Development Management Document.
- 5.7 16/05649/HOU - Two storey side extension, single storey rear extension, rear dormer, front rooflights, patio doors and Juliette balcony and extension of soil pipe at rear and erection of front entrance porch – Granted with conditions December 2016
- 5.8 16/04583/CEA - Extension to roof at side to form gable end with rear dormer, front rooflights, patio doors, balustrade, flank window and extension of soil pipe at rear. Single storey rear extension and extension to garages/outbuildings – Granted December 2016

6.0 Consultation

Statutory and Non-Statutory Consultees:

6.3 Internal Consultees:

Traffic & Transportation – Objection

The proposal does not meet the requirements of DMD 46 on the following points:

- The proposed two access points, one at the front of the property on Eversley Park Road and one to the side in Holly Hill are both within 10 metres of its junction with Eversley Park Road which is a classified road.
 - The corner location and street furniture serve to reduce sightlines and increase the likelihood of vehicle movements to and from the site being detrimental to highway safety at this junction. This would be exacerbated by vehicles entering and exiting the highway from the proposed vehicle accesses.
 - The proposal would involve the loss of the green space at the front of the property which will have a negative impact due to the loss of front garden and a garden wall.
 - There is existing off street parking space at the rear of the property via a vehicle crossover in Holly Hill which is more than 10 metres away from the junction with Eversley Park Road.
- 6.2 Highways – Object on grounds of highway safety as it is within 10 metres of junction which is contrary to adopted Council policy on the installation of vehicle crossovers

External Consultees:

- 6.3 None

Public

- 6.4 Consultation letters were sent to 5 neighbouring and nearby properties. One letter of objection had been received which raises in summary, the following points:
- Garage and hardstanding to rear of property which mitigates need for further parking to the front
 - Proposal would introduce dangerous movements close to a junction on a busy road and street furniture further restricting views
 - Neighbours 2 metre hedge also restricts views
 - Loss of front garden area
 - blue light primary route and bus route with bus stop in close along the road
 - Eversley Park Road is a walking route for Primary and Secondary Schools
 - Impact on both pedestrian and highway safety as vehicle use increases
 - Replacement tree which formed part of a planning approval would be lost.
 - Road sign would need to be relocated
- 6.5 The concerns raised by neighbouring occupier shall be covered in the report however the principle concerns are related to pedestrian and highway safety.

7.0 Relevant Planning Policies

7.1 The London Plan (2016)

- 6.2 Providing Public Transport Capacity and Safeguarding Land for Transport
- 6.3 Assessing Effects of Development on Transport Capacity
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing Traffic Flow and Tackling Congestion
- 6.12 Road Network Capacity
- 6.13 Parking
- 7.4 Local Character

7.2 Core Strategy (2010)

- CP25 Pedestrians and cyclists
- CP30 Maintaining and improving the quality of the built and open environment

7.3 Development Management Document (2014)

- DMD37: Achieving High Quality and Design-Led Development
- DMD45: Parking Standards
- DMD46: Vehicle Crossover and Dropped Kerbs

7.4 Other Material Planning Considerations

- National Planning Policy Framework (2019)
- National Planning Practice Guidance
- Council's Technical Footway Guidance 2013

8.0 Analysis

8.1 This report considers the issues that arise from the proposals having regard to National, Regional and adopted local planning policies and other material considerations.

8.2 The main issues for consideration are:

- Highway and pedestrian safety
- Design and Impact on the Character of the Surrounding Area

Previous Planning Decisions:

8.3 There have been three previous planning applications proposing crossovers to Eversley Park Road / Holly Hill: all of which have been found to be unacceptable on highway safety grounds and been refused planning permission. Two of the most recent decisions have been subject to an appeal process to the Planning inspectorate and in both cases, the Inspector has dismissed the appeals

upholding the Council's decision to refuse planning permission on the grounds identified.

- 8.4 In particular, the Inspector in determining the most recent appeal commented"

I have considered the appellant's personal requirements for requiring parking provision in front of the house rather than to the rear off Holly Hill but given that the proposed vehicular access would be close to the junction of Eversley Park Road and Holly Hill, this does not justify allowing a potentially dangerous arrangement. I consider that given the proximity of the proposed access to the junction, vehicles manoeuvring in this location could have a harmful effect on highway and pedestrian safety. This would conflict with Policy DMD 46.

- 8.5 Copies of these appeal decisions are attached as Appendices to this report but

- 8.6 There are no material differences between this and the two previous planning applications which were refused planning permission.

- 8.7 In assessing the previous planning applications it was considered that the formation of a vehicular access on either Eversley Park Road or Holly Hill to serve off street car parking for No. 47 Eversley Park Road where there is an existing alternative access to off-street parking from Holly Hill would lead to vehicles unnecessarily stopping, slowing down and turning from or onto the highway close to the junction of Holly Hill and Eversley Park Road. The presence of existing street furniture would further exacerbate the situation and serve to reduce sightlines and increase the likelihood of vehicle movement from the site being detrimental to highway safety at this junction. Since the previous application, there have been no material changes in circumstances and thus, it remains the considered opinion, consistent with the previous assessments, that the proposal would be contrary to the Council's Revised Technical Footway Guidance 2013, Policy DMD 46 of the Development Management Document, and the Core Strategy policies 24 and 30, as well as the aims of the NPPF.

Assessment against Policy DMD 46

- 8.8 The current proposal includes two vehicular accesses: one to Eversley Park Road and one to Holly Hill. The previous reasons for refusal and the planning inspector's decision were considered detrimental to highway safety due to the proximity to the junction. The current planning application does not overcome the previous reasons for refusal but simply increases the intensity of vehicle movements around the junction with two unacceptable vehicular accesses.

- 8.9 A full assessment has been undertaken including consultation with Traffic and Transportation along with our Highways Team both of which have objected to the scheme on highway safety grounds.

- 8.10 Policy DMD 46 sets out the criteria which would need to be met before a crossover onto a classified road can be approved:

- a) There is no negative impact on the existing character of the area and street scape as a result in the loss of a front garden or grass verges to hardstanding or loss of front garden walls;
 - b) There is no loss of street trees;
 - c) There is no increase in on street parking pressures in areas already experiencing high on-street parking demand as a result of introducing a vehicle crossover;
 - d) There is no adverse impact on the road safety;
 - e) There is no adverse impact on the free flow and safety of traffic on the adjoining highway and in particular on the effective movement of bus services;
 - f) Vehicles can enter / and exit the crossover in forward gear;
 - g) It has been shown that there are no alternative opportunities for safe access to the property (for example to the rear or side); and
 - h) The size of the off-street parking is large enough to ensure that vehicles do not overhang the footway.
- a) Impact on Appearance & Character of the area and street scape
- 8.11 The area is entirely suburban in character. Eversley Park Road is a classified road with a steep incline as you approach the application property from the south East. Other properties nearby have vehicle crossovers, but as confirmed by Transportation, these pre-date current policy which was adopted in November 2014. As such little weight is afforded to these as they were permitted under a different policy regime and this is supported by the recent appeals decisions which considered the weight to be assigned to this policy in light of local circumstances.
- 8.12 The proposed works would result in the loss of an existing mature boundary hedge and associated front landscaping which makes a positive contribution to the character of the area. Replacement hardstanding would virtually cover the whole front garden area with no visual relief.
- 8.13 It should also be noted that a large cherry tree which was removed as part of planning application and its replacement covered by condition is yet to be planted and no provision for the replacement has been shown on the submitted plans. The loss of the existing hedge and landscaping would have a negative impact on the streetscape. This fails to satisfy policy DMD37 and DMD46 (a) and Local Plan Policy CP30.
- b) There is no loss of street trees
- 8.14 The proposal does not involve the loss of a street tree
- c) There is no increase in on street parking pressures in areas already experiencing high on-street parking demand as a result of introducing a vehicle crossover
- 8.15 The level of on street parking is not an issue in this case

- d) There is no adverse impact on the road safety;
 - e) There is no adverse impact on the free flow and safety of traffic on the adjoining highway and in particular on the effective movement of bus services;
- 8.16 No. 47 is a corner plot and the application proposes two new vehicular crossings, one from Holly Hill and one from Eversley Park Road both of which are within 10 metres of the Eversley Park Road and Holy Hill junction. It must also be acknowledged that there is street furniture close to the junction, namely a street sign and a post box. The corner location and street furniture serve to reduce sightlines and increase the likelihood of vehicle movements from the site from either access being detrimental to highway safety at a junction where drivers would naturally be looking to turn or for vehicles turning rather for those appearing out of a drive. This is further exacerbated by the vehicle entering the highway from the proposed access point being required to perform a tight manoeuvre immediately on entering Holly Hill. Cars entering Holly Hill from Eversley Park Road would not be expecting a vehicle to be entering or exiting the site so close to the junction and visibility on this junction is hindered by the existing street furniture reducing visibility further detrimental to highway safety at this junction.
- 8.17 Automatic vehicle counts show that there are significant traffic volumes and speeds above the posted limit on Eversley Park Road. On weekdays between 8am and 9am there are, on average, 607 vehicles travelling along Eversley Park Road in the vicinity of the site. This equates to over one every six seconds.
- 8.18 These are high volumes and speeds for a residential road and mean that vehicles leaving the site in question will in all probability have a detrimental impact on road safety and the free flow of traffic.
- 8.19 It is therefore considered that given the proximity of the proposed accesses to the junction, location of street furniture and traffic volume, vehicles manoeuvring in this location could have a harmful effect on highway and pedestrian safety. This would conflict with Policy DMD 46 (d & e).
- f) Vehicles can enter / and exit the crossover in forward gear;
- 8.20 Setting aside the impact of the proposal on highway safety, in theory, the layout would permit this arrangement.
- g) It has been shown that there are no alternative opportunities for safe access to the property (for example to the rear or side);
- 8.21 The application site benefits from rear access via Holy Hill to a detached garage in the rear garden. There are no impediments to the continued access of this garage from Holly Hill and although the applicant's personal requirements for requiring parking provision in front of the house rather than to the rear off Holly Hill have been considered, it is considered they do not outweigh the harm caused to highway safety due to the proximity of the vehicular accesses to the junction of

Eversley Park Road and Holly Hill. It is considered given the alternative opportunities for off street parking, the proposal would not meet the criteria.

9.0 Conclusion

- 9.1 Taking the above considerations into account, it is clear the proposal would be contrary to criteria: a, d, e & g of DMD 46.
- 9.2 In particular, given the identified harm to highway safety, which was supported by the Planning Inspectorate together with the volume of traffic using the classified road, the fact that there is alternative and available access to off street parking in the rear garden, it is considered that the proposal would unacceptable and unnecessarily lead to vehicles stopping, slowing down and turning from or onto the highway close to the junction of Holly Hill and Eversley Park Road a classified road. This situation would be exacerbated by the profile of the road and the existing street furniture which would serve to reduce sightlines and increase the likelihood of vehicle movement from the site being detrimental to highway safety at this junction.
- 9.3 It should also be noted that separate Highways consent from the Council as Highway Authority would be required to construct the crossovers. This team have indicated that the proposal is contrary to their policy and would not receive approval.
- 9.4 The loss of existing established hedge and soft landscaping would also result in visual harm to the detriment of the street scene.
- 9.5 Overall therefore, the introduction of crossovers in this location has been consistently refused planning permission and on two occasions, this decision has been supported on appeal by the Planning inspectorate. In the absence of any change in circumstances, it is considered the proposal would be contrary to the Council's Revised Technical Footway Guidance 2013, Policy DMD 46 of the Development Management Document, and the Core Strategy policies 25 and 30, as well as the aims of the NPPF and planning permission be REFUSED.

Appeal Decision

Site visit made on 12 April 2018

by Julie Dale Clark BA (Hons) MCD DMS MRTPI

an Inspector appointed by the Secretary of State for Communities and Local Government

Decision date: 27 April 2018

Appeal Ref: APP/Q5300/D/18/3193446
47 Eversley Park Road, Southgate N21 1JJ

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
 - The appeal is made by Mr Murat Aydemir against the decision of the Council of the London Borough of Enfield.
 - The application Ref 17/03558/FUL, dated 7 August 2017, was refused by notice dated 20 October 2017.
 - The development proposed is vehicle access for off- street parking with partly block paving.
-

Decision

1. The appeal is dismissed.

Main Issue

2. I consider that the main issue is the effect of the proposal on highway and pedestrian safety.

Reasons

3. The appeal site is a semi-detached house on the corner of Eversley Park Road and Holly Hill and I note that it has an existing vehicular access off Holly Hill. There is street furniture close to the junction, namely a street sign and a post box. At the time of my site visit it was a fairly busy road and it is a bus route.
4. Policy DMD 46¹ sets out criteria for vehicle crossovers and dropped kerbs. It states that planning permission for new access onto 'A' roads and other busy classified roads will not normally be permitted. Eversley Park Road is not an 'A' classified road and although it is a busy road and a bus route I have no evidence to confirm that it falls within the definition set out in Policy DMD 46 in relation to the number of vehicles using the road.
5. However, the policy further sets out criteria where vehicle crossovers and dropped kerbs that allow for off-street parking and access onto roads will be permitted. These include where there is no adverse impact on road safety; there is no adverse impact on the free flow and safety of traffic on the adjoining

¹ Enfield Council Development Management Document (DMD), Adopted November 2014.

highway and in particular on the effective movement of bus services; vehicles can enter / and exit the crossover in forward gear; and it has been shown that there is no alternative opportunities for safe access to the property (for example to the rear or side).

6. I have considered the appellant's personal requirements for requiring parking provision in front of the house rather than to the rear off Holly Hill but given that the proposed vehicular access would be close to the junction of Eversley Park Road and Holly Hill, this does not justify allowing a potentially dangerous arrangement. I consider that given the proximity of the proposed access to the junction, vehicles manoeuvring in this location could have a harmful effect on highway and pedestrian safety. This would conflict with Policy DMD 46.
7. I have considered the Council's technical standards for footway crossovers², Core Strategy policies 24 and 30 and the National Planning Policy Framework³. I have also considered all other matters raised but none alter my conclusion. I conclude that the proposal would have a harmful effect on highway and pedestrian safety. It would conflict with Policy DMD 46 and therefore the appeal fails.

J D Clark

INSPECTOR

² Enfield Council Revised Technical Standards for Footway Crossovers (excluding Heavy Duty Crossovers), April 2013.

³ Department for Communities and Local Government National Planning Policy Framework, March 2012.



Appeal Decision

Site visit made on 6 March 2019

by J Alderman BA(Hons) MA MRICS

an Inspector appointed by the Secretary of State

Decision date: 16 May 2019

Appeal Ref: APP/Q5300/D/18/3218789

47 Eversley Park Road, Southgate, N21 1JJ

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
 - The appeal is made by Mr Murat Aydemir against the decision of Council of the London Borough of Enfield
 - The application Ref 18/03035/HOU, dated 31 July 2018, was refused by notice dated 25 September 2018.
 - The development proposed is a vehicular access with hard standing from Holly Hill and installation of electric car charging unit.
-

Decision

1. The appeal is dismissed.

Procedural Matter

2. The Council amended the description of development when validating the application to include a reference to the electric charging point. The appellant did not object to this. For clarity I have included the reference to the electric charging point in the description of the development given on the application form in the heading above.

Main Issue

3. The main issue is the effect of the proposal on highway safety.

Reasons

4. Eversley Park Road is a residential street with a mixture of property types. A number of the properties have crossovers and parking spaces within their front gardens. Holly Hill is a side street off of Eversley Park Road comprising mainly semi-detached properties. These properties generally have driveways and crossovers. Street parking is limited due to the width of the road and the double yellow lines each side of the splay of the road on the approach to the junction with Eversley Park Road.
5. On the day of my visit there was a regular flow of traffic along Eversley Park Road, with a range of different types of vehicles using the road, including buses. I also observed a number of vehicles entering and leaving Holly Hill.
6. The appeal property is located on the corner of Eversley Park Road and Holly Hill and has an existing garage at the end of the rear garden with what appeared to be a parking area in front of it. The garage and parking area are accessed from Holly Hill. On the approach, when turning left from Eversley Park

Road in to Holly Hill, immediately in front of the appeal property on Eversley Park Road there is a post box and a street sign.

7. The proposed parking space, which would be accessed from Holly Hill, would be created in what currently forms part of the front garden of the dwelling. From the plans provided and what I observed, the space would not be sufficient to enable a vehicle to enter and exit the space in forward gear, and no evidence has been provided to confirm otherwise.
8. The proposed access for the parking space would be located very close to the junction with Eversley Park Road. Given this, and the nearby street furniture, there would be limited visibility of, or from, the access. This would result in vehicles using it undertaking dangerous manoeuvres and creating a hazard for vehicles turning into Holly Hill, both of which would be detrimental to highway and pedestrian safety. The fact that vehicles would have to either reverse in or out of the parking space increases the likelihood of hazardous movements caused by the proposal.
9. Consequently, I consider that the proposal would be detrimental to highway safety. Therefore, it would not comply with Policy DMD 46 of the Development Management Document (adopted November 2014), Policies 24 and 30 of The Enfield Plan Core Strategy 2010 - 2025 (adopted November 2010) and the Revised Technical Standards for Footway Crossovers (April 2013) which, amongst other things, seek to safeguard highway safety for all users and ensure traffic flow is maintained.
10. I note and sympathise with the personal circumstances outlined by the appellant. However, it is unclear from the evidence provided the degree to which the revised parking arrangement would assist this resident, and whether other, less harmful, solutions have been explored that could accommodate these requirements. Moreover, personal circumstances seldom outweigh more general planning considerations and it is likely that the proposed development would remain long after the current personal circumstances cease to be material.
11. The requirements of the Lifetime Homes standards stated by the appellant are not relevant in this situation as, even if the dwelling has been refurbished and extended, it is not a new development. In addition, whilst it is stated by the appellant that the existing rear parking arrangements are not safe, no evidence has been provided to substantiate this claim.
12. The appellant, through the traffic survey they carried out, point to limited traffic movements in Holly Hill. However, even if the traffic movements are limited, due to the location of the proposed parking space, this would not sufficiently overcome the potential hazard caused.
13. In support of the appeal my attention has been drawn to a number of other properties whose front gardens are used for parking and which have vehicular accesses across the pavement. However, I do not have the full details of the circumstances of these cases, including their planning status or the development plan policies that applied at the time of their consideration. Moreover, many of the examples do not occupy corner plots, or appear to share the same locational characteristics as the appeal site. Accordingly, their circumstances are not directly comparable to those which apply in this appeal. I have, in any case, determined the appeal on its own merits

14. Whilst I note that No 2a Holly Hill has a driveway located in similar proximity to the junction with Eversley Park Road, the creation of an additional access at a similar point in the road on the opposite side only increases the potential hazard for passing traffic.

Conclusion

15. The appellant has stated that the proposal would benefit a resident of the dwelling who, due to age and health considerations, is unable to manage the current parking arrangements. In my assessment of the appeal I have therefore had due regard to the Public Sector Equality Duty (PSED) contained in section 149 of the Equality Act 2010, which sets out the need to eliminate unlawful discrimination, harassment and victimisation, and to advance equality of opportunity. The proposed development would assist a person who shares a protected characteristic for the purposes of the PSED. However, based on the evidence before me, I cannot conclude that the appeal scheme would necessarily offer the only solution to meeting the resident's requirements and this lessens the benefit arising from the proposal.

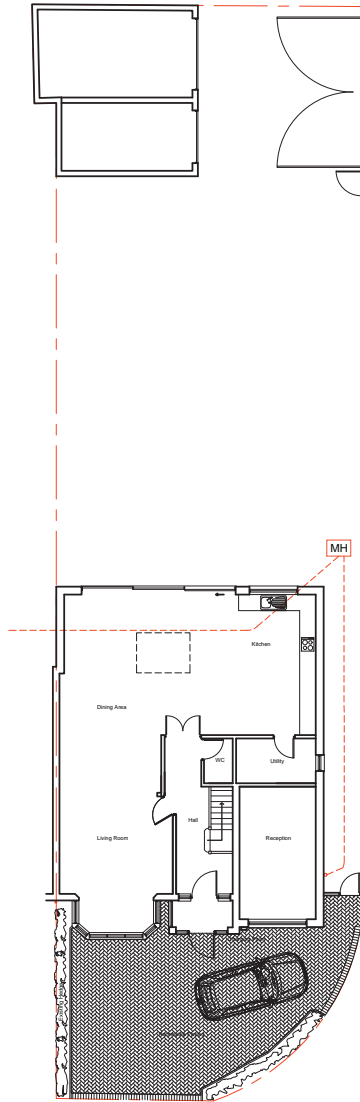
16. It does not follow from the PSED that the appeal should succeed. In this case the development would cause harm and be detrimental to highway safety. As such, it would be contrary to the development plan. In its favour it would provide a facility that would benefit an elderly occupier of the dwelling. However, on balance, I consider this benefit would not outweigh the planning objections to the proposal.

17. Therefore, for the reasons given above, I have concluded that the appeal should be dismissed.

J Alderman

INSPECTOR

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 Scale: 1:200



Site Plan

Notes

All dimensions to be checked by the Contractor before construction proceeds.

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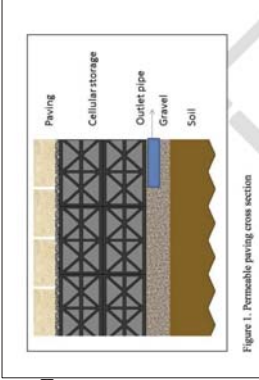
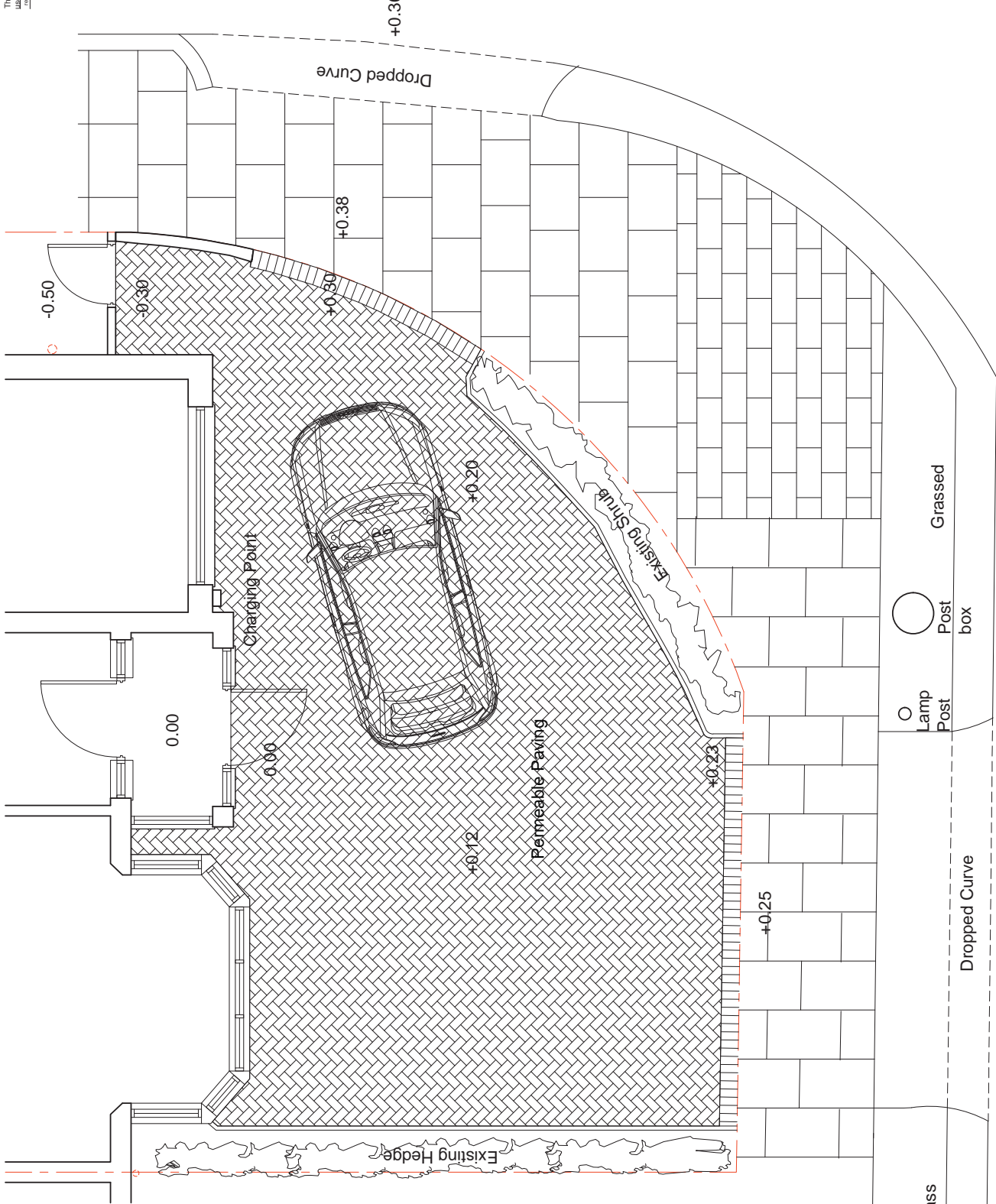
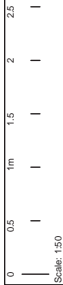
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 Intelligent Architecture and Surveying 47 Eversley Park Road, London N21 1JJ T: 0208 4435882 M: 07956 296 076 E: murat@i-arch.co.uk W: www.i-arch.co.uk	
project: 47 Eversley Park Road London N21 1JJ	
title: Proposed Site Plan	
scale: 1:200 @ A3	dim: MA
date: May 2019	
drg no. 1279-200	rev

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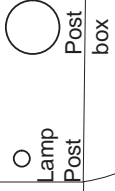
project: 47 Eversley Park Road
London N21 1JJ

title: Proposed Site Plan
scale: 1:50 @ A3
date: May 2019

drawn: MA
checked: chk
ref: 1279-210

Dropped Curve Detail

FOR PLANNING



Grass

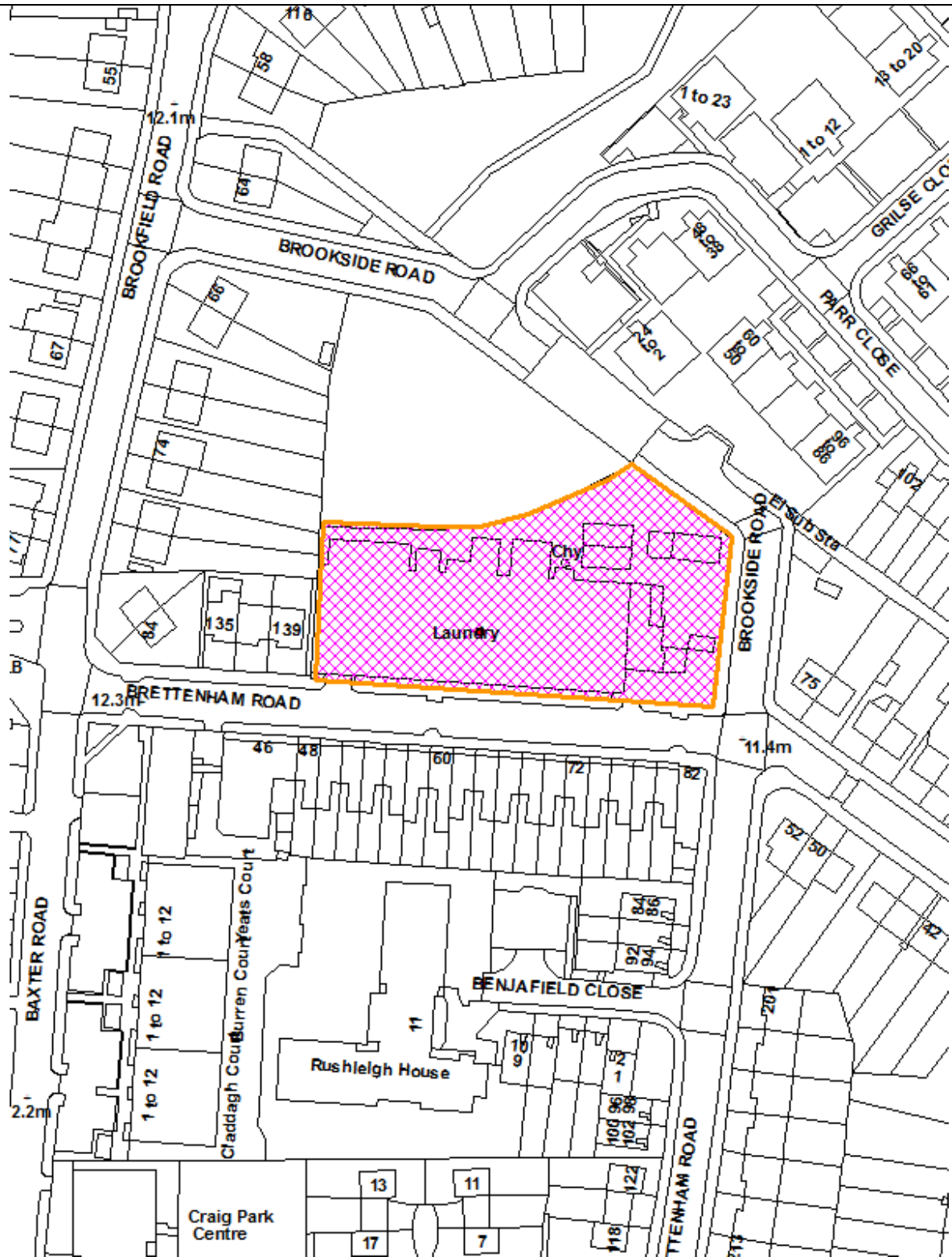
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Dropped Curve

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LONDON BOROUGH OF ENFIELD		
PLANNING COMMITTEE		Date: 15 October 2019
Report of: Head of Planning	Contact Officer: Andy Higham Evie Learman 0208 132 0873	Ward: Edmonton Green
Application Number: 19/00986/FUL		Category: Major
LOCATION: Sinclair's Laundry Site, 199 Brettenham Road, London N18 2HE		
PROPOSAL: Demolition of existing buildings and erection of replacement two-storey building for industrial laundry use involving new salt and water tanks, widening of existing vehicular access, new fence to side and rear boundaries and associated works.		
Applicant Name & Address: Sinclair's Laundry The Imperial London Hotels Limited		Agent Name & Address: Collective Planning 1 Long Lane London SE1 4PG
RECOMMENDATION: That subject to the completion of a S106 Agreement to secure the obligations as set out in the report, the Head of Development Management / the Planning Decisions Manager be authorised to GRANT planning permission subject to conditions.		

Ref: 19/00986/FUL LOCATION: Laundry Site, 199 Brettenham Road, London, N18 2HE



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North



1. Recommendation

1.1 That subject to the completion of a S106 Agreement to secure the obligations as set out in the report, the Head of Development Management / the Planning Decisions Manager be authorised to **GRANT** planning permission subject to conditions:

1. Time Limited Permission

2. Drawing Numbers

Development to be carried out in accordance with approved plans.

3. Materials

Sample panel and a schedule of materials to be used in all external elevations including walls, doors, windows and front entrances within the development.

4. Landscaping

i) Details of trees, shrubs, grass and all other soft landscaped areas of internal and external amenity spaces to be planted on the site;

ii) Where feasible biodiversity enhancement interventions incorporated into the design; and

iii) Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details.

5. Tree Protection - Provident Park

Scheme for the protection of the trees (in the root protection area) in Provident Park, in accordance with BS5837:2012, including a tree protection plan (TPP) and an Arboricultural method statement (AMS)

6. Operational/Service Management Plan

Operational/Service Management Plan including details of:

i) How delivery vehicles will be managed to ensure there is no occurrences of vehicle idling in close proximity to the site;

ii) How vehicle deliveries will be managed to ensure there is no undue noise and disturbance to neighbouring occupiers as a result of the deliveries;

iii) Confirmation that loading doors will be closed shut prior to any vehicle being unloaded in the site; and

iv) Procedure for dealing with noise complaints arising from neighbouring occupiers.

7. BREEAM

Evidence that development achieves a BREEAM New Construction rating of no less than 'Very Good'.

8. Details of access and junction reinstatement/modification

Details of the amended vehicle accesses associated with the development, as follows:

- i) Reinstatement of kerbs on two crossovers on Brookside Road;*
- ii) Widening of existing kerb/gates on Brookside Road;*
- iii) Minor relocation of existing (non-service) dropped kerb on Brettenham Road; and*
- iv) Extension of the existing footway crossovers on Brettenham Road and Brookside Road.*

9. Previously Unidentified Contamination

10. Piling, Deep Foundations, and Boreholes

11. Construction Management Plan (CMP)

Construction Management Plan in accordance with London Best Practice Guidance.

12. Cycle Storage

Details of the siting and design of secure covered cycle parking facilities.

13. Subdivision

Building shall not be occupied by more than one business at any one time.

14. Nesting Boxes

Details of bird and/or bat nesting boxes/bricks.

15. Carbon Reduction

The development shall be implemented in accordance with the submitted Energy Statement.

16. Energy Performance

Energy Performance Certificate accompanied by a Built Energy Performance Assessment shall be submitted.

17. Green Procurement and Construction Plan

Green Procurement and Construction Plan detailing how the development has sought to minimise the environmental impact of the scheme.

18. Surfacing Materials

Details and design of the surfacing materials to be used within the development including footpaths, shared surfaces, access roads, parking areas and road markings.

19. Pedestrian Pathways

Details of pedestrian pathways within the curtilage of the site.

20. SUDS – Drainage Strategy

Sustainable Drainage Strategy shall be submitted.

21. SUDS – Verification Report

22. Water Pollution

23. Acoustic Report

Acoustic report to be submitted.

24. External Lighting

2. Executive Summary

2.1 The report seeks approval to a scheme involving the demolition of existing buildings and erection of a replacement two-storey building for industrial laundry use involving new salt and water tanks, widening of existing vehicular access, new fence to side and rear boundaries and associated works. The existing laundry building has been operating on the site for approximately 100-years and the applicants seek to remain at the site but can only do so if expansion and upgrade of existing works occurs.

2.2 The reasons for recommending approval are:

- i) The proposed use would remain as existing which is a well established, acceptable light industrial use;
- ii) The proposed development would be consistent with the objectives of national, regional and local policy in terms of supporting and securing sustainable growth and employment opportunities within the borough;
- iii) The development would improve the quality of the laundry facility's operations in terms of being able to better control noisy operations and vehicular activity (deliveries);
- iv) The upgrade of the site would retain 55 full-time jobs within the borough;
- v) The development would improve the local environment and deliver improved boundary treatment with the adjoining open green space as well as delivering improvements to the open green space secured by legal agreement
- vi) The existing somewhat dilapidated building would be greatly improved and provide a visual upgrade to the immediate street scene and wider surrounding area;
- vii) In comparison to the existing facility the new building would be significantly more sustainable and energy efficient;
- viii) The development would create 7 on-site parking spaces (including a disability space) which would reduce impact upon on-street parking.

24 on-site cycle spaces would also be created which would encourage sustainable methods of transport. As such the development is acceptable in terms of parking, traffic generation and servicing.

- 2.3 The proposed works include demolition of the chimney which would result in the loss of a local landmark however the chimney is not listed and has no other protection mechanisms afforded to it.
- 2.4 The existing site area is 3326 sqm with the existing gross internal area being 2628 sqm. The application proposes an additional 851 sqm of gross internal floorspace taking the total internal floor area to 3479 sqm.

3. Site and Surroundings

- 3.1 The site, measuring 0.3315ha, comprises a group of one and two storey light industrial buildings (Use Class B1c) with the main orientation and access points to the site facing south onto Brettenham Road. The existing buildings are dated and in a poor condition. The laundry has been operating on the site since 1918.
- 3.2 In between Brookside Road to the north and the site is an area of public green space. A terrace of residential properties lie to the west with the rear gardens abutting part of the site. Further residential properties lie to the south and west with No.139 Brettenham Road immediately adjacent to the west of the site.
- 3.3 The main orientation and accesses points to the site face south onto Brettenham Road. A staff car park area is accessed from fenced gates to the north-east of the site.
- 3.4 The site has no designations within the Local Plan however the area of public green space to the rear is designated as Local Open Space.
- 3.5 The immediate surrounding area is largely characterised by residential use with properties in a variety of architectural styles.
- 3.6 The site does not contain any listed building's and does not lie within a Conservation Area. The site falls within Flood Zone 2 and 3.
- 3.7 The existing industrial laundry is under the ownership of The Imperial London Hotels Ltd, who has managed and operated the laundry for the past 100 years to serve their central London hotels. The existing laundry largely employs local people and currently receives around 6 deliveries per day operating a 24/7 system.
- 3.8 There are no existing restrictions on servicing and operational hours and the facility would continue to be a 24/7 operation. The existing use of the site is B1c and this would be retained.

4. Proposal

- 4.1 The proposal is for the demolition of existing buildings and erection of a replacement two-storey building for industrial laundry use involving new salt and water tanks, widening of existing vehicular access, new fence to side and rear and associated works. More specifically the proposal comprises:

- Demolition of existing buildings;
- Construction of new building which will also extend to the eastern boundary providing a two-storey element;
- Rationalisation of the roof form and facades to provide more visual coherency;
- Soft landscaping to periphery;
- Provision of two (2) new 10m high water tanks located externally to the rear/north and measuring approximately 10m high
- Erection of new palisade 2.5m high fencing to the rear and sides.

- 4.2 The laundry facility will continue to operate a 24/7 facility with three rolling shifts per day and up to 25 people on site at any given time (with a total maximum number of people employed at the site numbering 55.
- 4.3 The existing use of the site is B1c (light Industrial) and this would be retained.
- 4.4 The current application has been through the formal pre-application process with the Council prior to the current submission.
- 4.5 The pre-application response is summarised as follows:
- The proposal considered acceptable in principle;
 - Some minor modifications to the design are encouraged to ensure some level of visual interest to the street is achieved;
 - Further consideration as to whether the chimney could be retained as it could be viewed as a local landmark;
 - Measures to reduce noise in the form of building fabric and/or 'quiet measures' in the form of equipment is supported;
 - The number of vehicle crossovers should be minimised in order to decrease the potential for highway conflict;
 - The provision of car and cycling parking is supported; and
 - Proposed trees along Brettenham Road may not be achievable given the restricted space on the footpath.

5. Relevant Planning History

- 5.1 PA/02/0032 – Installation of ancillary equipment cabin.
- 5.2 PA/02/0015 – Installation of 15m high telecommunication mast with equipment cabin and ancillary equipment.
- 5.3 TP/98/1002/1 – Details of loading/unloading, fume extraction and sound insulation submitted pursuant to conditions 2, 3, 4 of TP/98/1002 (REFUSED).
- 5.4 TP/98/1002 – Two-storey infill extension to sorting room (GRANTED 23/09/1998)

6. Summary of Key Reasons for Recommendation

- i) The principle of this industrial development is established with the existing use;
- ii) The development would contribute to and retain employment opportunities, largely for local residents;

- iii) The development is acceptable for this location in terms of its appearance, size, siting, scale and design and is an improvement to the existing building;
- iv) The development provides seven formal car parking spaces and 24 cycle parking spaces (the existing site provides neither);
- v) The development does not have an unacceptable impact on neighbouring residential amenity and the upgrade in facilities and provision of a building more fit for purpose will go some way to reducing and containing noise within the building;
- vi) The periphery of the site will be visually improved in terms of a number of dropped kerbs being removed and pavements reinstated. Improvement works to Provident Park at the rear of the site will also take place with new tree planting secured by way of the Section 106 agreement;
- vii) Vehicular movements will be better controlled within and outside of the site with a more cohesive vehicle movement system and enclosed loading bay as well as restricted delivery times; and
- viii) The sustainability credentials of the building will be improved by use of measures such as: high performance building fabrics; water efficiency measures; low airtightness to reduce heat losses through infiltration further; maximising natural light by introducing light panels in the wall cladding; and low energy light fittings with sophisticated controls including daylight and occupancy sensors.

7. Consultation

Public Response:

7.1 Consultation letters were sent to 476 neighbouring properties and a press advert was placed in the local newspaper. Seven responses have been received and these are summarised as follows (officer response follows in para 7.2 and further discussion is within the analysis section of the report):

- 1. Noise generating from the operation of the facility (machinery noise and delivery vehicles);
- 2. Loss of light from the enlargement of the building;
- 3. Loss of parking;
- 4. Increase in pollution arising from any changes to salt-based works; and
- 5. Congestion arising from delivery vehicles waiting to access the site.

7.2 Officer response to neighbour comments as follows:

- 1. The facility will be upgraded in terms of the building envelope. This will result in the building having improved insulation to mitigate noise and an improved and more sophisticated equipment within the facility which will also improve noise reduction. Furthermore, the Environmental Health Officer has raised no objection to the proposal and has advised that a condition is attached to any permission requiring the submission of an acoustic report to confirm the predicted noise reduction.

The use of the laundry will be intensified however the improved construction of the building and upgraded internal facilities will mean

that even with the intensification of the use there will be no additional noise generated over and above the existing.

In terms of deliveries the development proposes an increase from 6 to 12 deliveries per day however the proposed improvement to vehicle access and egress within the site will mean there will be limited if any additional disturbance arising from the extra deliveries over and above existing. Notwithstanding a condition is recommended requiring a management plan to be submitted with details of how delivery vehicles will be managed to ensure there are no waiting delivery vehicles in close proximity to the site. In addition, a further condition is recommended restricting vehicle deliveries/pick-ups to be between the hours of 0600-1800 Monday to Friday and 0800-1600 on Saturdays with no deliveries/pick-ups permitted Sundays and bank holidays.

2. The height of the building will increase from between 7.395m to 10.7m (highest point on the eastern side building) to around 9.56m across the site. As such, whilst the height will increase in parts from the existing building it will decrease in other parts. Notwithstanding, where there is an increase the parts of the building that will increase in height are considered to be insufficient enough to result in any impact on light to neighbouring occupiers.
3. The site will provide seven formal parking spaces compared to the zero as is the current situation. Parking matters are discussed further within the main body of the report.
4. The relevant submitted information has been reviewed by the Council's Environmental Health Officer who has raised no objections subject to conditions.
5. A new delivery system will be implemented which will ensure delivery vehicles are better managed. This is further discussed in the main body of the report.

External Consultees:

- 7.3 *Thames Water:* No objection. Requested condition to be attached requiring technical details of the connection to the existing foul water network infrastructure to accommodate the needs of the development proposal.
- 7.4 *MPS Designing Out Crime:* No objection. Condition requiring the development to confirm attainment of Secured by Design Accreditation, is requested. (Officer comment: there is no explicit policy requirement requiring a Certificate of Compliance to be achieved and therefore a condition will not be attached. However, it should be noted that the majority of the Secure by Design interventions can be implemented on the site).
- 7.5 *London Fire Brigade:* No objection.
- 7.6 *Environment Agency:* No objection.

Internal Consultees:

- 7.7 *Traffic & Transportation:* No objection subject to conditions pertaining to cycle storage required and vehicle crossovers/dropped kerbs.
- 7.8 *Tree Officer:* No objection.
- 7.9 *SuDS Officer:* No objection. Conditions pertaining to SUDS required.
- 7.10 *Environmental Health Officer:* No objection. Conditions pertaining to noise report and Construction Management Plan required.

8. Relevant Policies

8.1 London Plan (2016)

- 2.6 Outer London: vision and strategy
- 2.7 Outer London: economy
- 2.8 Outer London: transport
- 2.16 Strategic outer London development centres
- 3.1 Ensuring equal life chances for all
- 4.1 Developing London's economy
- 4.4 Managing industrial land and premises
- 4.12 Improving opportunities for all
- 5.1 Climate change mitigation
- 5.2 Minimising carbon dioxide emissions
- 5.3 Sustainable design and construction
- 5.5 Decentralise energy networks
- 5.6 Decentralised energy in development proposals
- 5.7 Renewable energy
- 5.8 Innovative energy technologies
- 5.9 Overheating and cooling
- 5.10 Urban greening
- 5.11 Green roofs and development site environs
- 5.12 Flood risk management
- 5.13 Sustainable drainage
- 6.3 Assessing effects of development on transport capacity
- 6.9 Cycling
- 6.10 Walking
- 6.11 Smoothing traffic flow and tackling congestion
- 6.12 Road network capacity
- 6.13 Parking
- 7.1 Lifetime neighbourhoods
- 7.2 An inclusive development
- 7.3 Designing out crime
- 7.4 Local character
- 7.5 Public realm
- 7.6 Architecture
- 7.14 Improving air quality
- 7.15 Reducing and managing noise, improving and enhancing the acoustic environment and promoting appropriate soundscapes

8.2 The London Plan – Draft

A draft London Plan was published on 29 November 2017 for consultation purposes with a deadline for consultation of 2 March 2018. The draft plan is a material consideration in determining applications but is likely to carry little or no weight until there is a response to consultation submissions or until after its examination. Of particular relevance is Policy GG5 (Growing a good economy); Policy D2 (Delivering good design); and Policy E6 (Locally Significant Industrial Sites)

8.3 Core Strategy

SO1	Enabling and focusing change
SO2	Environmental sustainability
SO6	Maximising economic potential
SO7	Employment and skills
SO8	Transportation and accessibility
SO10	Built environment
CP9	Supporting community cohesion
CP13	Promoting economic prosperity
CP16	Taking part in economic success and improving skills
CP24	The road network
CP25	Pedestrians and cyclists
CP26	Public transport
CP27	Freight
CP30	Maintaining and improving the quality of the built and open environment
CP32	Pollution

8.4 Development Management Document

DMD 20	Locally Significant Industrial Sites
DMD 21	Complementary and supporting uses within SIL and LSIS
DMD 22	Loss of employment outside of designated areas
DMD 23	New employment development
DMD 37	Achieving high quality and design-led development
DMD 38	Design process
DMD 39	The design of business premises
DMD 45	Parking standards and layout
DMD 46	Vehicle crossovers and dropped kerbs
DMD 47	Access, new roads and servicing
DMD 48	Transport assessments
DMD 50	Environmental assessment methods
DMD 51	Energy efficiency standards
DMD 53	Low and zero carbon technology
DMD 55	Use of roof space/ vertical surfaces
DMD 56	Heating and cooling
DMD 57	Responsible sourcing of materials, waste minimisation and green procurement
DMD 58	Water efficiency
DMD 64	Pollution control and assessment
DMD 65	Air quality
DMD 66	Land contamination and instability
DMD 68	Noise
DMD 69	Light pollution

DMD 80 Trees on development sites

8.5 Other Material Considerations

- National Planning Policy Framework (NPPF) 2018 (revised)
- National Planning Practice Guidelines (NPPG)
- Enfield Characterisation Study
- Manual for Streets
- Mayors Transport Strategy (May 2010)
- Revised Technical Standards for Footway Crossovers (April 2013)
- Refuse and Recycle Storage Guide Enfield (ENV 08/162)
- Travel Planning for new development in London 2011 (TfL)
- Manual for Streets 1 & 2, Inclusive Mobility 2005 (DfT)

9. Assessment

9.1 The main issues arising from this proposal for Members to consider are:

1. Principle;
2. Design;
3. Amenity;
4. Transport;
5. Refuse, Waste and Recycling;
6. SuDS;
7. Sustainability;
8. Biodiversity;
9. Trees;
10. Planning Obligations; and
11. Community Infrastructure Levy.

Principle of Development

- 9.2 The existing industrial facility is in some state of disrepair and does not function in a sustainable or effective manner. The development proposes to improve the functionality of the building including increasing the employment floorspace. The works would also result in significant improvements to the external appearance of the building.
- 9.3 The site is not protected in terms of containing a listed building or being located within a Conservation Area. Given the condition of the existing buildings and the lack of protection afforded to the site the principle of demolition is considered acceptable.
- 9.4 The site primarily employs local people and the applicant has stated there would be no loss of jobs arising from the development. The works will result in an increase of 851sqm of gross internal floorspace and provide an improved working environment for employees.
- 9.5 The external works will provide improved visual amenity to surrounding residential occupiers via the provision of a new and better designed facility. There will also be an upgrade to the boundary to the adjacent area of green space with new fencing and shrub planting.
- 9.6 In terms of land use, London Plan Policy 2.7 seeks to to address constraints and opportunities in the economic growth of outer London so that it can rise

above its long-term economic trends. In addition, London Plan Policy 4.1 seeks to promote and enable the continued development of a strong, sustainable and increasingly diverse economy across all parts of London, and support and promote outer London as an attractive location for national government as well as businesses.

- 9.7 Core Strategy Policy 13 notes that the Council will protect and improve the borough's employment offer and will seek to maintain the competitiveness of employment sectors that are well established and successful in Enfield by providing support and investment to existing companies to facilitate them remaining in the borough. Meanwhile Core Strategy Policy 16 seeks to promote and protect local employment whilst Policy DMD 22 seeks to protect employment by resisting the loss of or reduction of employment within the borough.
- 9.8 The site lies within the Upper Lee Valley Opportunity Area (referred to as the Lee Valley Opportunity Area in the draft London Plan), where it has been identified as a strategic location for industrial, business and employment. As the draft London Plan seeks to continue the role of industrial functions in the area, the proposed continuation of the industrial building will be in line with the adopted and emerging London Plan.
- 9.9 Furthermore the industrial facility will continue to provide and slightly increase employment opportunities to the local area which will contribute to the indicative employment capacity of 15,000 jobs within the Upper Lee Valley Opportunity Area.
- 9.10 The proposal would be wholly consistent with these policies. Furthermore, the comprehensive redevelopment of the site would raise the visual quality of the area and provide an industrial building fit for purpose. The scheme will therefore involve the redevelopment of a prominent industrial site, provide investment in terms of significantly upgrading the building and retain local employment.
- 9.11 It is clear therefore that in principle, the redevelopment of the site is wholly compatible with national, regional and local policy. As such, given the significant improvements that would occur as a result of the development together with there being no loss of employment, the application is supported in principle, subject to further assessment of any design, amenity and/or transport impacts.

Design and Appearance

- 9.12 In terms of design, Core Strategy Policy 30 requires all developments to be high quality and design led, having special regard to their context. Whilst Core Strategy Policy 9 requires proposals to promote attractive, safe, accessible, inclusive and sustainable neighbourhoods as well as connecting and supporting communities and reinforcing local distinctiveness.
- 9.13 Meanwhile Policy DMD 37 seeks to achieve high quality design and requires development to be suitably designed for its intended function that is appropriate to its context and surroundings. The policy also notes that development should capitalise on opportunities to improve an area and sets out urban design objectives relating to character, continuity and enclosure,

quality of the public realm, ease of movement, legibility, adaptability and durability, and diversity.

- 9.14 In terms of business premises making efficient use of land and maximising their contribution to the urban environment, Policy DMD 39 outlines a list of design criteria which proposals for business premises should seek to adhere to, including the requirement to positively address the public realm; clearly differentiate between public and private areas; provide inclusive access arrangements; sensitively screen and locate servicing, parking and refuse (to the rear where possible), mitigate potential negative impacts in surrounding uses; ensure massing and facades are visually interesting; respect the character of the surrounding area; and use high quality materials that can create, enhance or preserve the local character and identity.

Legibility / Character

- 9.15 The existing site has been an operational laundry for 100 years and although it is accepted that the majority of buildings and structures are of limited architectural merit, with the exception of the admin block that is considered to be a good art deco example of its type, the historic value of the site is still considered significant within the local area. In this respect, the existing 25m high chimney provides a landmark signifying the continuing industrial use of the site, with its prominence enhanced by the green space to the north that provides greater views of it than otherwise would be the case if buildings surrounded it on all sides. However, it is accepted that as the building is not a designated or non-designated heritage asset there is no obligation for the applicants to retain this feature which would in any event look incongruent within the scope of the new building.

Height, Bulk and Massing

- 9.16 The proposed buildings will be part single, part two-storey's in height and will overall be slightly higher than the existing building with the current facility measuring between 7.395 to 9.56m high with the apex of the proposed building measuring approximately 9.95m high. The existing building includes a 25m high chimney which will be removed as part of the works. The new building will also rationalise the existing jagged shaped roof by providing a flat roof which would provide a more uniform and consistent appearance. This will result in a building with a more discreet appearance, which is supported given its close proximity to neighbouring residential properties.
- 9.17 The proposed buildings will be part single, part two-storey's in height and will overall be slightly higher than the existing building however this will result in a more cohesive design which will sit better than the existing building within the streetscene. In addition, the flat roof form will align better with the nearby residential buildings which are largely 2-storey.

Appearance

- 9.18 The proposed building will continue to maintain an industrial appearance which will support its function and identity. In terms of materiality, opaque daylight panels (i.e. light will still be able to filter through) are proposed on the front and rear elevations in place of existing brick and painted render walls. This will add to the visual interest in the front and rear elevations whilst ensuring privacy is retained to neighbouring occupiers.

- 9.19 The building will also comprise cladding and bricks which would be in keeping with the nature of the building and the surrounding residential property. As these materials will cover large areas, a condition is recommended requiring further details are submitted to ensure high quality materials are used which would enhance the appearance of the building.

Public Realm Improvements

- 9.20 The site currently has existing wire boundary fencing up to 2.0m high along the north (rear) and eastern side of the site. This fencing to the rear will be replaced with palisade fencing measuring 2.5m high to improve security as well as provide a visual uplift to the existing fencing whilst the fencing to the east will be removed. As the footprint of the proposed building extends to the boundary, the existing wire fence will be replaced by the facades of the new building which will provide visual interest as well as security.
- 9.21 Other public realm improvements include the removal of an existing low-level wall on the Brettenham Road elevation which will enable the pavement to become wider to improve the pedestrian environment. There will also be some improvements to the public open green space to the north of the site. These improvements are to be agreed any may incorporate items such as the provision of bench/es and some planting. They will be provided by a financial contribution £2,500 which will be secured within the Section 106 agreement.

Summary of Design and Appearance

- 9.22 The proposed development is industrial in scale, design and language. Whilst the building lies within a residential area it nonetheless would retain its industrial identity. Given the building will replace an existing industrial facility its replacement would still be considered to be in keeping with the character and appearance of the surrounding townscape. As the rationalised design would result in a more cohesive appearance it would be an improvement in terms of visual amenity and provide an improvement to the streetscene in comparison to the existing situation.
- 9.23 In light of this context, it is considered that the proposed building and works to the immediate surrounding area result in a much improved and better quality building than the existing. Overall the proposal is considered to be a well-designed development that will significantly improve the appearance of the current outdated and in some places, dilapidated, facility. Given the close proximity of the residential properties in the immediate area the improvements to the area of green space to the north of the site are welcomed and where relevant, these will be secured within the legal agreement.
- 9.24 Given the above the proposal is considered acceptable in terms of design and appearance.

Amenity

- 9.25 London Plan Policy 7.6 states that buildings should not cause unacceptable harm to residential amenity, including in terms of privacy and overshadowing. Policies DMD 6 and 8 ensure that residential developments do not prejudice the amenities enjoyed by the occupiers of neighbouring residential properties in terms of privacy, overlooking and general sense of encroachment and the

principles contained in this policy have been applied in this case given the relationship to residential properties. Furthermore, Policy CP30 of the Local Plan seeks to ensure that new developments have appropriate regard to their surroundings, and that they improve the environment in terms of visual and residential amenity.

- 9.26 The site is located in a predominantly residential area in close proximity to neighbouring occupiers with a distance of around 15.5m between the southern façade of the site and the residential properties lying opposite on Brettenham Road. However, as the laundry has been operating on the site since 1918 it is considered that the facility is well established within the locality.

Operational Hours / Deliveries

- 9.27 There is currently no restriction on the operating hours of the facility, and this will remain the case with the new facility, i.e. the facility will continue to operate on a 24-hour basis 7-days a week.
- 9.28 Whilst there is no restriction on the operational hours of the facility, the upgrade of the laundry will result in an intensification of the use which will subsequently lead to an increase in deliveries. The existing facility has approximately 6 vehicle deliveries in a 24-hour period, mainly during the day, and this will increase to approximately 12 deliveries in a 24-hour period. Whilst the deliveries to the new facility will also be largely during the day, there will be occasions where deliveries will be during the evening and/or night.
- 9.29 The upgrade of the laundry facilities and the subsequent intensification of the use could, as mentioned above, also result in a further increase in deliveries in the future. Given the size and capacity of the laundry it is not expected that any increase in delivery numbers would be substantial however the nature of the business means that the laundry needs to be quick to respond to the laundry needs of the hotels that they service and whilst the majority of laundry deliveries are dealt with during the day, the laundry does need to retain the ability to be able to deliver during the night.
- 9.30 The current facility has a fleet of 3 twelve-tonne lorries, two of which have been upgraded to quieter running gas powered vehicles with the remaining vehicle and any additional future vehicles planned to also be upgraded in the near future. These new vehicles are expected to go some way to facilitating quieter deliveries as will the new delivery access arrangement, which will include a one-way delivery system within the facility with the entrance doors closing after vehicles have entered the facility.
- 9.31 Given the above it is considered there would not be any undue adverse amenity impact arising to neighbouring occupiers in terms of the operational hours and/or deliveries.

Overlooking / Privacy

- 9.32 As mentioned above the new building will include daylight panels on the front elevation facing onto Brettenham Road however these will be opaque to ensure the privacy of neighbouring occupiers is retained whilst still allowing daylight into the building. As such it is considered there would not be any

undue adverse impact arising to neighbouring occupiers in terms of light spillage and /or light pollution.

Light Pollution

- 9.33 As the laundry operates on a 24-hour basis internal blackout shutters are proposed on the eastern and southern elevations in order to mitigate light spillage to neighbouring occupiers.

Noise

- 9.34 The new building will be built to a far higher specification than the existing including new roof and cladding which are more fit for purpose in terms of sound insulation and functionality and as such noise spillage will be reduced by the improved external envelope. In addition, the laundry proposes noise minimising features such as the replacement of the existing metal trolley system with PVC trolleys with low noise wheels.
- 9.35 As mentioned above delivery vehicles will enter and exit differently to the existing system which will reduce vehicle movements along Brettenham Road. In addition, the proposed new vehicle loading bay will be enclosed within the building, further containing noise to internal areas.

Summary

- 9.36 Whilst it is noted that there is some level of concern from neighbouring occupiers in relation to existing noise and disturbance becoming worse, it is clear from the above outlined measures that the opposite will occur. That is, the proposed new measures in terms of vehicle movements and drops-offs, and improved building fabric and internal noise mitigation measures, will result in a quieter facility, despite the intensification of the use.
- 9.37 Notwithstanding the above a condition is recommended requiring further details in the way of an acoustic report to ensure that the sound level generated from the combined plant does not exceed acceptable levels.
- 9.38 In light of the above the proposal is considered acceptable in terms of residential amenity impact subject to conditions as stated.

Highways Impact

- 9.39 DMD 45 relates to car parking, cycle provision and parking design. DMD 47 states that new development proposals will need to demonstrate that enough space for servicing, circulation and access to, from and through the site is provided. All developments must be fully accessible to pedestrians and cyclists and assist with general permeability within an area and the current factory does not provide this. London Plan policy 6.13, DMD policy 45 (Parking Standards and Layout) and 47 (Access, New Roads and Servicing) states that operational parking for maintenance, servicing and deliveries is required to enable a development to function.
- 9.40 The Public Transport Accessibility Level (PTAL) of the site is 2 which is low. Brettenham Road is within a CPZ however restrictions apply to THFC 'event days' only between 12noon and 9pm.

Access / Egress

- 9.41 The site currently contains three dropped kerbs on Brookside Road. Two of these dropped kerbs will be reinstated to a pedestrian pathway and one (at the most northern point of the site) will be widened. There are also two existing dropped kerbs on Brettenham Road, and both of these will be retained (one for servicing purposes only); the other dropped kerb will be moved slightly and widened.
- 9.42 The changes to the dropped kerbs are to facilitate easier access and egress within the site. To this end a route is proposed to run north to south through the site and the building from the consolidated Brookside Road access to the existing eastern Brettenham Road access, which will act as the exit for service vehicles.
- 9.43 The submission documents state that service vehicles will drive through the building, side-unloading dirty laundry, whilst simultaneously side-loading laundry in a single manoeuvre within the boundary of the site. The associated manoeuvres have been demonstrated by a submitted swept path analysis and are considered satisfactory.
- 9.44 The provision of the through route and side-loading capability removes the need for service vehicles to perform reverse manoeuvres into the eastern and western Brettenham Road accesses in order to unload soiled laundry and reload clean laundry respectively. At the eastern Brettenham Road, the access will be widened by approximately 2.3 metres making the exit manoeuvre easier for service vehicles. This is therefore, considered an improvement on the existing situation.
- 9.45 In addition to the above the proposal also includes the removal of a low wall along the site frontage, which will result in an improvement in terms of visibility for service vehicles. This will also increase the width of the pavement along the frontage of Brettenham Road and as such improve the path for pedestrians.
- 9.46 Finally by placing the loading/unloading of laundry within the building and ensuring that doors are closed after vehicles have entered, noise and visual impacts to neighbouring residents will be further minimised, which is also considered an improvement to the existing situation.

Delivery Hours

- 9.47 Paragraphs 10.3 to 10.7 above outline the existing and proposed arrangements in terms of deliveries in relation to potential impact on residential amenity. However, to summarise there is no current restriction on the operational hours of the facility and the operational hours will continue unrestricted after the facility has been upgraded. The unrestricted operational hours are also applicable to deliveries (as is currently the case) and the laundry is expected to have up to 12 deliveries in a 24-hour period (including overnight), 7-days a week.
- 9.48 Although the use of the laundry will be intensified and the number of deliveries increase from 6 to 12 in a 24-hour period, the increase in vehicles is not expected to result in undue transportation issues. This is because the

new facility will incorporate a new one-way delivery system which will stop the need for vehicles reversing out of the facility. Although it is not possible to predict specific delivery times, as these vary according to the changing needs of the operation and the hotels that are serviced, a Service/Operational Management Plan is required to be submitted with details of how delivery vehicles will be managed. This Plan should include confirmation that vehicles will use the 1-way delivery system; that loading bay doors will be shut before vehicles are unloaded; and that no waiting vehicles will be permitted to idle in nearby residential streets whilst they are waiting to enter the laundry.

Car Parking

- 9.49 The parking provision of 7 spaces (1 per 500sqm) is in accordance with the standards set out in The London Plan, which requires the provision of 1 space per 100-600sqm. Currently there is no on-site parking provision however the applicant has stated that of the 52 workers currently employed by the laundry; 45 live locally to the site and commute to work via sustainable means (83%).
- 9.50 The applicants have also confirmed that whilst the remaining members of staff drive to work, and currently park on-street in the surrounding areas, this on-street parking has not given rise to any particular highway safety concerns. The provision of 7 on-site parking spaces therefore represents a significant improvement over the existing situation of zero on-site parking spaces. Furthermore, the development will not result in an increase in the number of employed members of staff and this proposal will not worsen the current situation.
- 9.51 In terms of staff travelling to the site at any given time the laundry will operate over three daily shifts, with a day shift (06:00-14:00), an evening shift (14:00-22:00) and a night shift (22:00-06:00). The applicant has confirmed that there would be a maximum of 25 workers on site at any given time, with the total number of people employed on site numbering 54 (although the submitted planning documents state there would never be this amount of people on site at any one time). As previously mentioned, the application documents state that approximately 83% of the laundry workers use public transport or walk or cycle. As such it is not considered there would be an unacceptable pressure on on-street parking in the surrounding streets, particularly as 7 new on-site car parking spaces will also be provided. These will be located at the rear of the site and accessed via the deliveries entrance on Brookside Road.
- 9.52 The application proposes to retain a dropped kerb for maintenance purposes however it is noted that its removal would add an additional on-street parking space. Notwithstanding, the applicant has stated that the dropped kerb would be used only on a weekly basis for maintenance and servicing of the laundry machinery and would not be used for daily deliveries or collection of laundry which will be carried out at the vehicle drive though at the other end of the building. Whilst it would be preferable to have an additional on-street car parking space in this location it's removal would result in significant operational issues for the laundry in terms of being able to maintain machinery and equipment. As the proposal does not really change the exiting situation and includes other measures towards improving the public realm, such as soft landscaping around the site, it is considered there will be an overall improvement to the existing public realm condition in any event. In light of the above the retention of the dropped kerb is considered acceptable.

Cycle Parking

- 9.53 In terms of cycle parking, the visitor cycle parking is to be located in the main cycle store on the ground floor nearest to the Brettenham Road pedestrian entrance as shown on drawing '14E7.LO.1000.A rev 12'. However, whilst the cycle parking location and quantum is considered acceptable, it is recommended that further details on cycle parking, specifically where visitor and long stay cycle parking will be located within the allocated cycle parking area, is required via condition.

Summary

- 9.54 The above assessment demonstrates that the proposal would not result in an unacceptable impact in terms of traffic and transportation matters, and furthermore is not expected to result in any significant additional impact over and above the existing. As such, subject to conditions requiring a Service/Operational Management Plan to be submitted and a condition providing full details of cycle storage to also be submitted, the proposal is considered acceptable in terms of traffic and transportation.

Refuse, Waste and Recycling

- 9.55 Policy 5.17 of the London Plan requires suitable waste and recycling storage facilities in all new developments whilst Core Policy 22 supports the provision of a sufficient, well-located waste management facility and requires all new developments to provide on-site waste treatment, storage and collection throughout the lifetime of the development.
- 9.56 Meanwhile Policy DMD 57 notes that all new developments should make provision for waste storage, sorting and recycling, and adequate access for waste collection.
- 9.57 With regards to the new laundry facility a waste management facility is proposed to the rear of the site which will cover the requirements of the laundry operation. The application documents confirm that Imperial London Hotels Ltd collect and manage their own waste and do not require collection services from the Council.
- 9.58 Given the above the application is considered acceptable in terms of refuse, waste and recycling.

Sustainable Drainage / Flood Risk

- 9.59 London Plan policies 5.12 and 5.13 require the consideration of the effects of development on flood risk and sustainable drainage respectively. Core Policy 28 ("Managing flood risk through development") confirms the Council's approach to flood risk, inclusive of the requirement for SuDS in all developments. Policy DMD59 ("Avoiding and reducing flood risk") confirms that new development must avoid and reduce the risk of flooding, and not increase the risks elsewhere and that planning permission will only be granted for proposals which have addressed all sources of flood risk and would not be subject to, or result in unacceptable levels of flood risk on site or increase the level of flood risk to third parties.

- 9.60 DMD61 (“Managing surface water”) requires the submission of a drainage strategy that incorporates an appropriate SuDS scheme and appropriate greenfield runoff rates.
- 9.61 The application has been negotiated during the course of the assessment to ensure that it meets necessary Council requirements. Conditions are though recommended requiring a Final SUDS Drainage Strategy and a SUDS Verification Report to ensure approved drainage/ SUDS details have been fully implemented.

Sustainability

- 9.62 The NPPF confirms that the purpose of the planning system is to contribute to the achievement of sustainable development, and policies relevant to sustainability are set out throughout the NPPF. Further planning policies relevant to sustainability are set out in chapter 5 of the London Plan, which states that development proposals should make the fullest contribution to minimising carbon dioxide (CO₂) emissions in accordance with the following energy hierarchy:
- Be Lean: use less energy;
 - Be Clean: supply energy efficiency; and
 - Be Green: use renewable energy.
- 9.63 Enfield’s DMD policy 49 requires the highest sustainable design and construction standards, having regard to technical feasibility and economic viability. These policies require new developments to address the causes and impacts of climate change by minimising energy use, supplying energy efficiently and using energy generated from renewable sources (Core Strategy Policy 20 and DMD51), seeking zero carbon developments (DMD50), using decentralised networks where feasible (DMD52), and providing on-site renewable energy generation to make-up any shortfall where feasible (DMD53).

District Energy Network

- 9.64 The application site lies within approximately 900m of the planned network and as such a future connection to a District Heating scheme may be feasible.
- 9.65 There is expected to be a network nearby in 2022 (provided by ‘Energetik’) however Energetik have confirmed that, subject to the opportunity being reasonable for them, they could provide a temporary boiler if there is a time lag between the development being operational and the provision of the nearby network around 2022.
- 9.66 The application documents state that the energy centre of the building is designed such that once a district heating network becomes available close to the site, the building could connect to the network, subject to viability and financial feasibility. Given the ever increasing importance of sustainable development, in order to ensure the possibility of connecting to a DEN is fully explored a Heads of Term has been included in the legal agreement. This would also align with the developments other sustainability measures such as sourcing washing and drying products that will drive down carbon in excess of 28%. A connection to a DEN could save up to 80% carbon for heat supplied and as such should be fully explored in terms of viability/feasibility.

- 9.67 The development of industrial buildings of this scale is expected to achieve improvements in line with the Building Regulations (Part L). The Energy and Sustainability Statement confirms that the new building will exceed 35% carbon reduction. To ensure the energy performance is in accordance with Building Regulations, a condition is attached.
- 9.68 DMD Policy 50 requires non-residential development to be BREEAM 'Excellent' rating (73.71%). The applicant has submitted an Energy and Sustainability Statement stating the building fabric of the proposal will be compliant with Building Regulations Part L and BREEAM 'Very Good' rating. The submission documents state that due to the way BREEAM ratings are calculated the proposed development could not achieve an 'Excellent' rating despite offering the following strategies (which do not form part of the BREEAM rating system):
- An inherent part of running a laundry is using a significant amount of water continuously for the operation. The new laundry plant and equipment are designed to reuse the wasted water, and use the water more efficiently, therefore reduce the water consumption significantly by at least 50%;
 - The laundry currently has a license to extract borehole water which services the building. The extracted water is already softened and checked frequently for quality. Therefore, this reduces its reliance on using potable mains water;
 - Replacing existing laundry diesel service vehicles with new low CO2 LPG vehicles with increased load capacity to reduce trip numbers; and
 - Non-scheduled small vehicle deliveries will be handled by electric vehicles already in operation by The Imperial London Hotels Ltd.
- 9.69 The applicants state that the above measures combined will have a considerably more positive impact on the environment and sustainability than all other BREEAM issues combined.
- 9.70 Other active energy efficiency features include:
- High performance building fabric of low U-values that exceed Part L minimum standards;
 - Low airtightness to reduce heat losses through infiltration further;
 - Maximising natural light by introducing light panels in the wall cladding;
 - High efficiency building services systems utilising gas-fired condensing boilers for space heating and hot water;
 - Fresh air will be provided via Mechanical Ventilation units with Heat Recovery that preheat incoming air with the wasted heat of the exhaust air; and
 - Low energy light fittings with sophisticated controls including daylight and occupancy sensors.
- 9.71 Whilst low/zero carbon technologies to provide heat or generate energy on site will include:
- A VRF system utilising reverse-cycle Air Source Heat Pumps of high efficiency will provide both space heating and comfort cooling in the

office areas. An ASHP will also be used to provide the base annual hot water load;

- Photovoltaic panels installed on the roof will generate electricity on site.

9.72 Subject to the above measures being incorporated into the development and the viability of becoming connected to a District Energy Network being fully explored as required by the section 106 agreement, the proposal is considered acceptable in terms of sustainability and energy matters.

Biodiversity

9.73 Through Policy 36 of the Core Strategy the Council commits to 'protect, enhance, restore or add to biodiversity interests within the Borough'. This is reaffirmed in the DMD policies 78 to 81.

9.74 The National Planning Policy Framework (NPPF) recognises that the planning system should aim to conserve and enhance the natural and local environment by minimising impacts on biodiversity and providing net gains in biodiversity where possible, contributing to the Government's commitment to halt the overall decline in biodiversity, including the establishing of coherent ecological networks that are more resilient to current and future pressures. Paragraph 175 of the NPPF also states that opportunities to incorporate biodiversity in and around developments should therefore be encouraged.

9.75 The application site is situated in a highly urbanised and industrial environment. The site is dominated by the existing laundry facility and parking / servicing areas. As a result, the site has little biodiversity or ecological value at present.

9.76 It is considered there would be a biodiversity enhancement as part of an overall landscaping scheme which is to be conditioned. The proposal allows for landscaping works to the area of green space to the rear/north of the site and potentially to the front elevation on Brettenham Road.

Trees

9.77 Policy DMD 81 states development must provide high quality landscaping that enhances the environment. The development will not include the removal of any trees however the applicant has agreed to pay a financial sum for the provision of tree planting in the public Provident Park at the rear of the site (outside of the red line). The species of trees will be chosen by the Council's arboriculturists and the Council will also undertake the physical planting. As Provident Park currently has a low level of trees in relation to its size additional tree planting would be considered to add positively to the environmental quality of the area. It is noted that Secure by Design comments have previously advised that trees and shrubs are not planted near to the boundary in order to keep the area open and not provide concealment.

Secure by Design

9.78 Following consultation with the Metropolitan Police Service (MPS) Designing out Crime team, the project has the potential to meet some of the criteria for Secured by Design Accreditation. It is noted that part of the criteria involves erecting a highly visible boundary fence a metre higher (to 3.0m in total)

which the Council considered to be unacceptable in visual amenity terms given the residential location of the site. As such the height of the perimeter fence has been lowered to 2.5m and this will be positioned to a small section of the western boundary, the whole of the northern boundary and part of the eastern boundary. Although the MPS recommend a condition that all other recommendations in relation to Secured by Design good practice are implemented this is not considered appropriate as there is no explicit policy requirement requiring a certificate of compliance. As such the proposal is considered acceptable in this regard.

10. Planning Obligations

10.1 The necessary Heads of Terms are:

- Financial contribution of £2,500.00 towards improvement to green space (specifically tree planting) to north of the site;
- Employment and skills strategy in accordance with S106 SPD;
- District Energy Network viability information to be submitted; and
- Monitoring fee

11. CIL

11.1 This would be calculated in accordance with the Mayor's adopted Community Infrastructure Levy Charging Schedule 2012 and Enfield's adopted Community Infrastructure Levy Charging Schedule 2016. The payments would be chargeable on implementation of the commercial development.

11.2 Based on the existing use being an industrial laundry (Use Class B1) and the proposed use remaining the same and an increase in floor area of 851sqm x £60 = £51,060.

12. Conclusion

12.1 The proposed upgrade of the existing laundry building and site is welcomed in principle, and the application has been considered with regard to the National Planning Policy Framework (NPPF) and its presumption in favour of sustainable development.

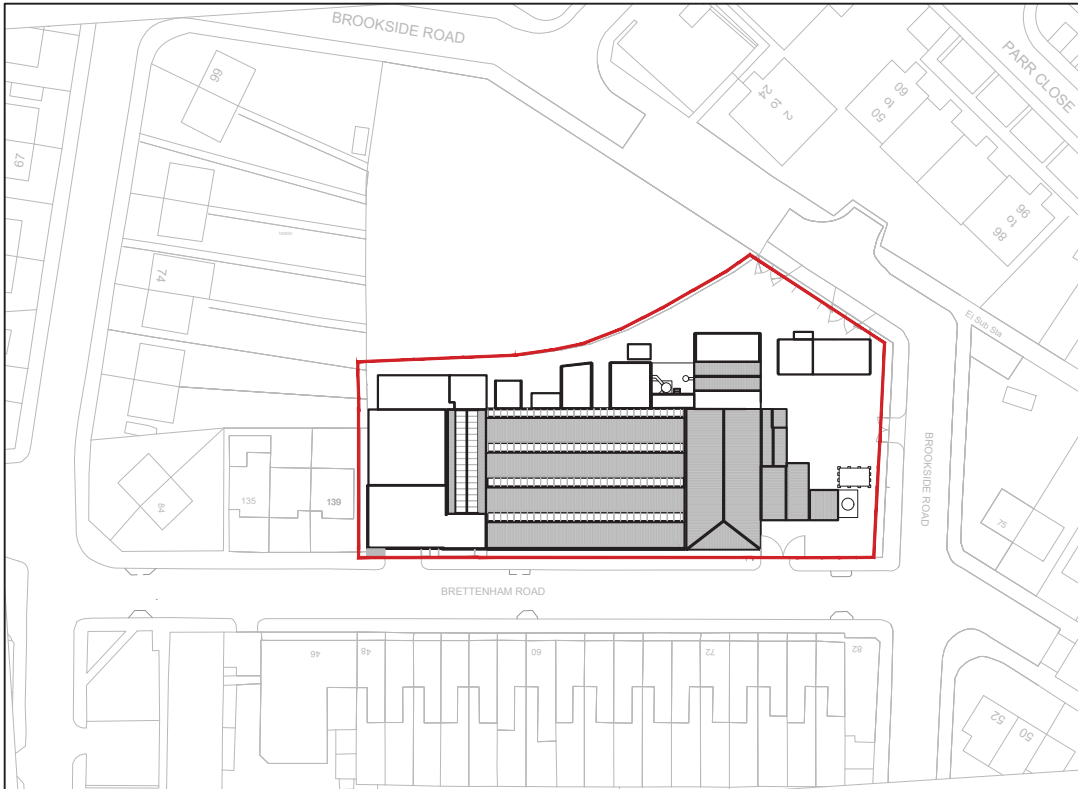
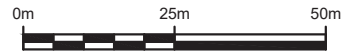
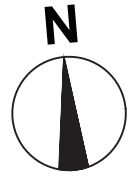
12.2 The upgrade of the site will ensure its' sustainability in terms of being fit for purpose going forward. The site provides ongoing employment largely for local residents which is consistent with a number of Council corporate priorities and the prevailing Development Plan policies in the London Plan and Core Strategy and is a key material planning consideration to be weighed up as part of the assessment of the application.

12.3 The proposal is considered acceptable in terms of land use, which is already established, and is also considered acceptable in terms of design, neighbour amenity impact, transport impact, proposed sustainability and energy reduction measures. This is subject to conditions and to an appropriate Section 106 (s106) agreement, the draft Heads of Terms of which have been agreed with the applicant.

12.4 This report shows that the benefits of the proposed development have been given due consideration and are sufficient enough to outweigh any perceived harm. In this respect the benefits are summarised again as follows:

- The land use is already established with the existing use;
- The development contributes to and retain employment opportunities, largely for local residents;
- The development improves the appearance of the existing building and is considered appropriate in terms of its appearance, size, siting, scale and design;
- The development provides seven formal car parking spaces and 24 cycle parking spaces (the existing site provides neither);
- The development is acceptable in terms of impact on neighbouring occupiers and the new building incorporates noise reduction measures and as such will be a better neighbour than the existing in terms of noise;
- The periphery of the site will be visually improved in terms dropped kerbs being removed and pavements reinstated. Improvement works to Provident Park at the rear of the site will also take place with new tree planting secured by way of the Section 106 agreement;
- Vehicular movements will be better controlled within and outside of the site with a more cohesive vehicle movement system and enclosed loading bay as well as a vehicle management plan; and
- The sustainability credentials of the building will be improved by use of measures such as: high performance building fabrics; water efficiency measures; low airtightness to reduce heat losses through infiltration further; maximising natural light by introducing light panels in the wall cladding; and low energy light fittings with sophisticated controls including daylight and occupancy sensors.

12.5 Having regard also to the mitigation secured by the recommended conditions and Section 106 Agreement, it is considered the proposed development is acceptable when assessed against the suite of relevant planning policies and that planning permission should be granted.



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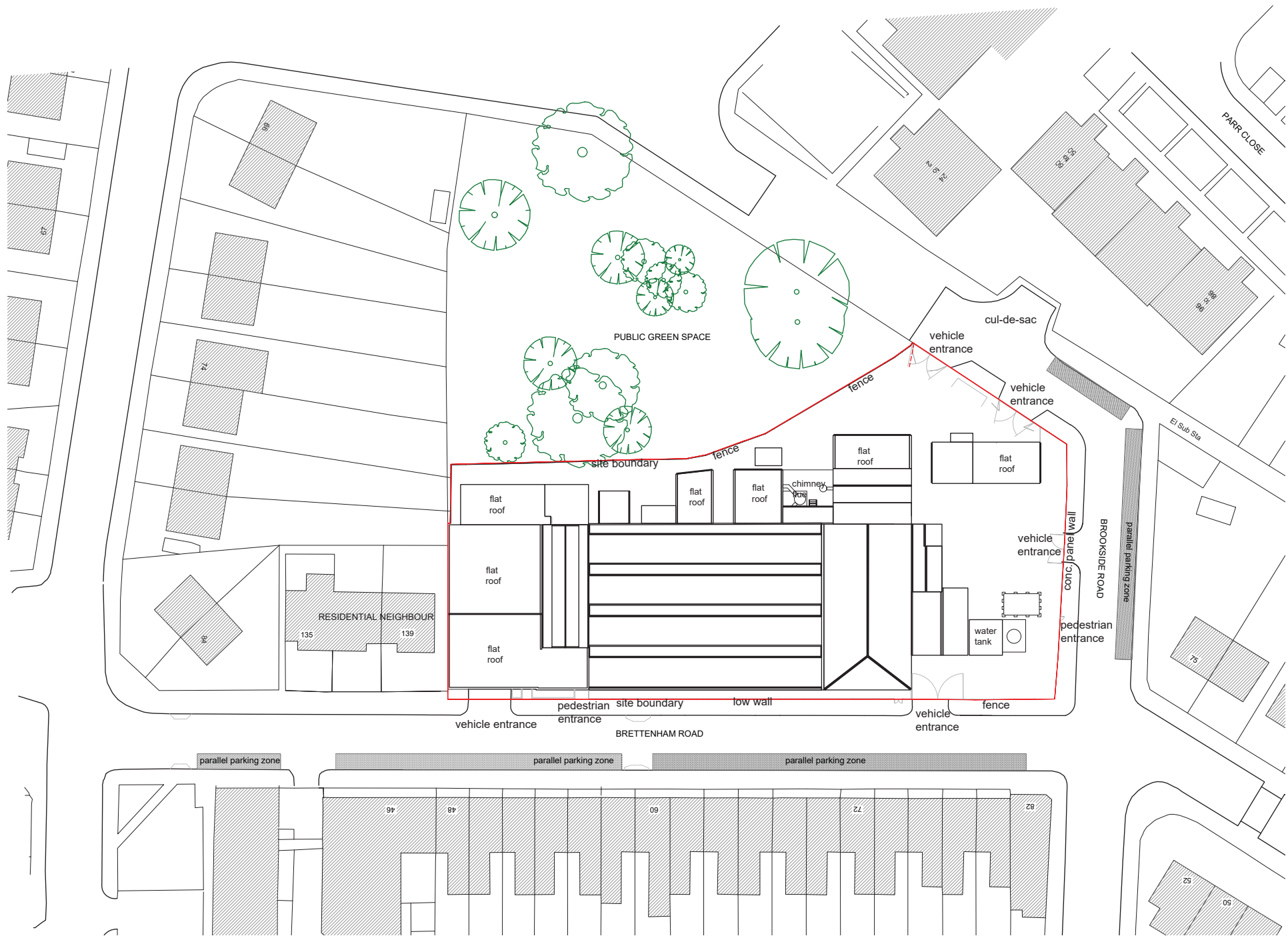
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SITE LOCATION PLAN

Scale: 1:1250 @A4

architects
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Project INPERIAL LONDON HOTELS SINCLAIRS LAUNDRY, EDMONTON NORTH LONDON, N18 2HE	Date 30/03/2015	Drawn by RVW	00 * PLANNING	30.03.2015 RVW
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Drawing Title SITE LOCATION PLAN	Drawing No. 14E7.EX.001	Rev 00	rev	amendments
	Cad File 14E7.LO.001	date		by
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rev amendments date by

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Project
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Drawing Title
**EXISTING
 SITE BLOCK PLAN**

Drawing Status
PLANNING

Date 05.03.2019 Drawn by RVW

Scale A3 @ 1:500

Drawing No. 14E7.EX.002 Rev 00

Cad File 14E7.LO.002

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SITE BLOCK PLAN - EXISTING

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Page 93



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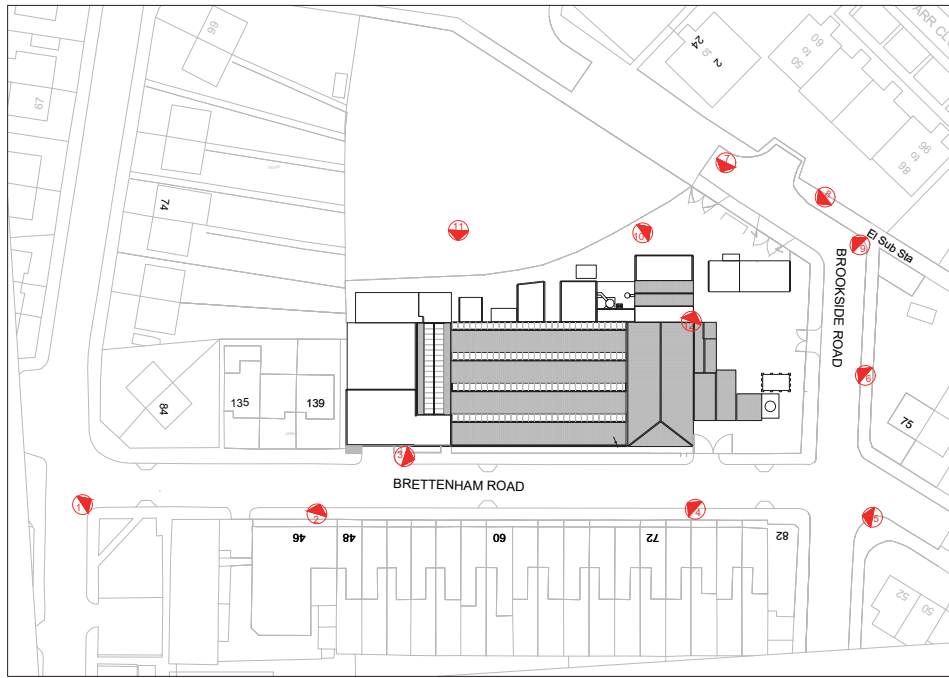
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* Planning			
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* Planning			
00	27.06.2018		
* Preliminary issue for review			
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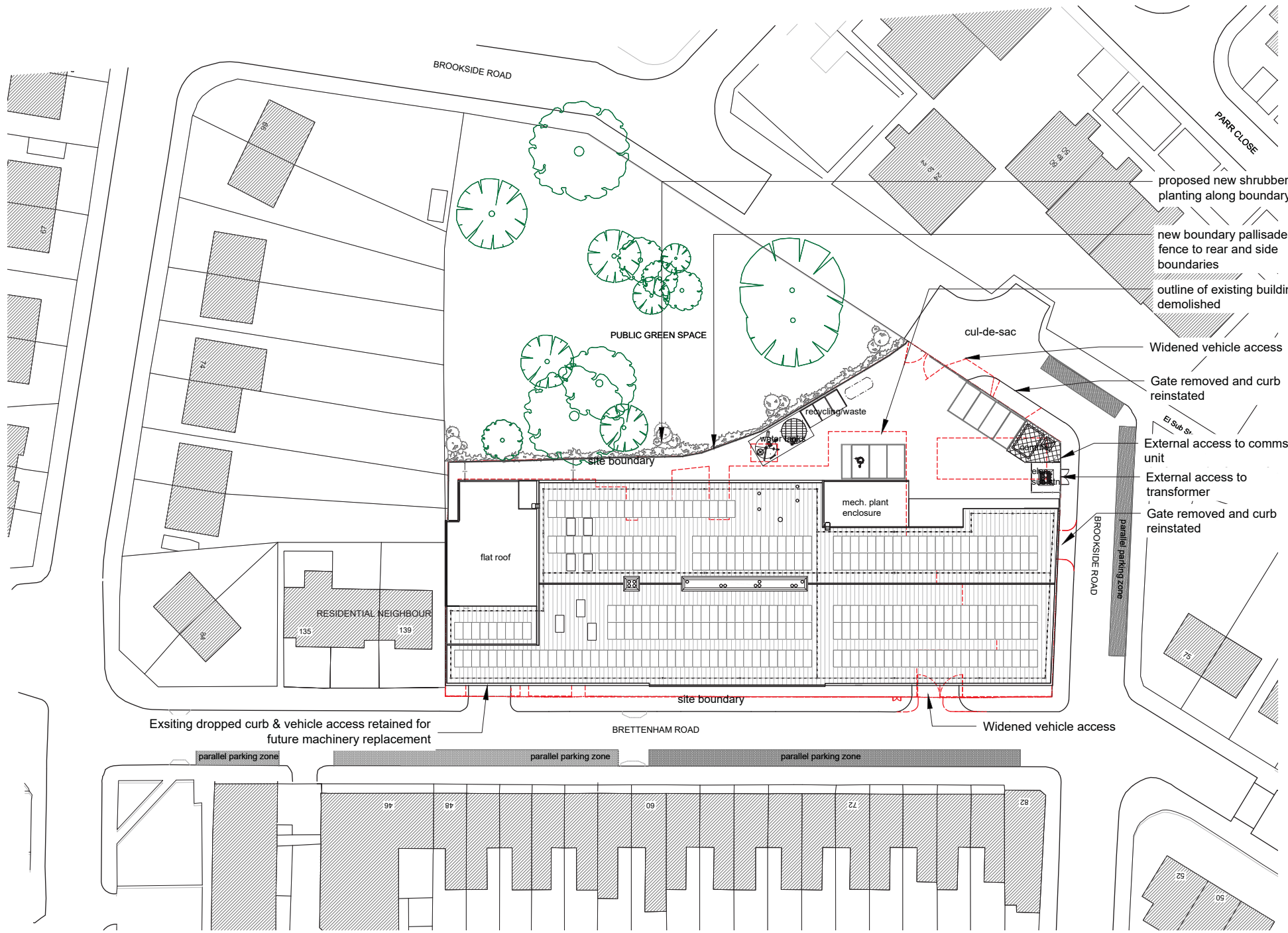
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- proposed new shrubby planting along boundary fence
- new boundary pallisade fence to rear and side boundaries
- outline of existing building demolished
- Widened vehicle access
- Gate removed and curb reinstated
- External access to comms unit
- External access to transformer
- Gate removed and curb reinstated

00 05.03.2019
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rev amendments date by

Pages 9/95



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Drawing Title
PROPOSED
SITE BLOCK PLAN

Drawing Status
PLANNING

Date 05.03.2019 Drawn by RVW

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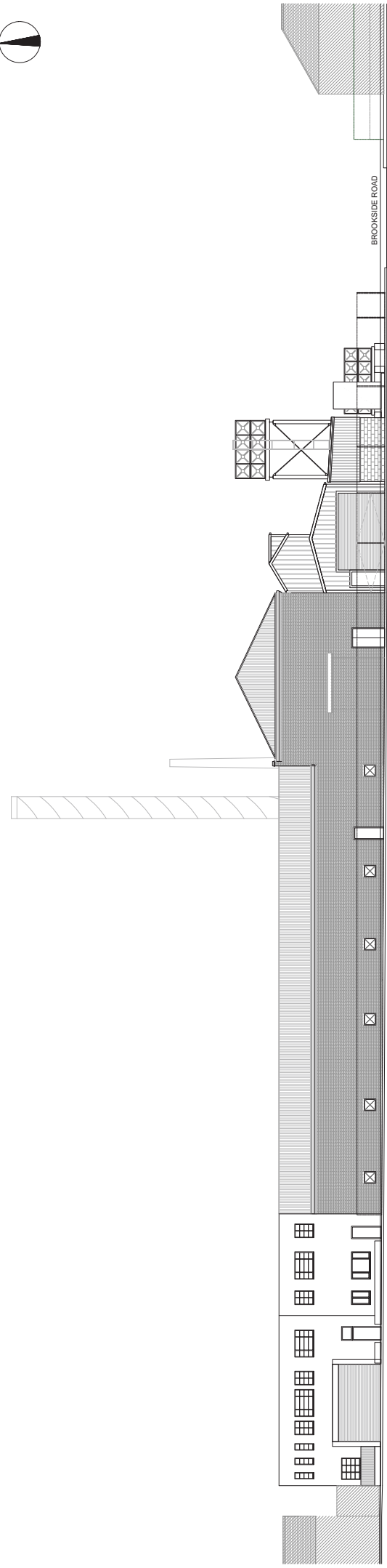
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1 SITE BLOCK PLAN - PROPOSED
 Scale: 1:500@A3



BROOKSIDE ROAD

1 SOUTH ELEVATION

Scale: 1:100 A0

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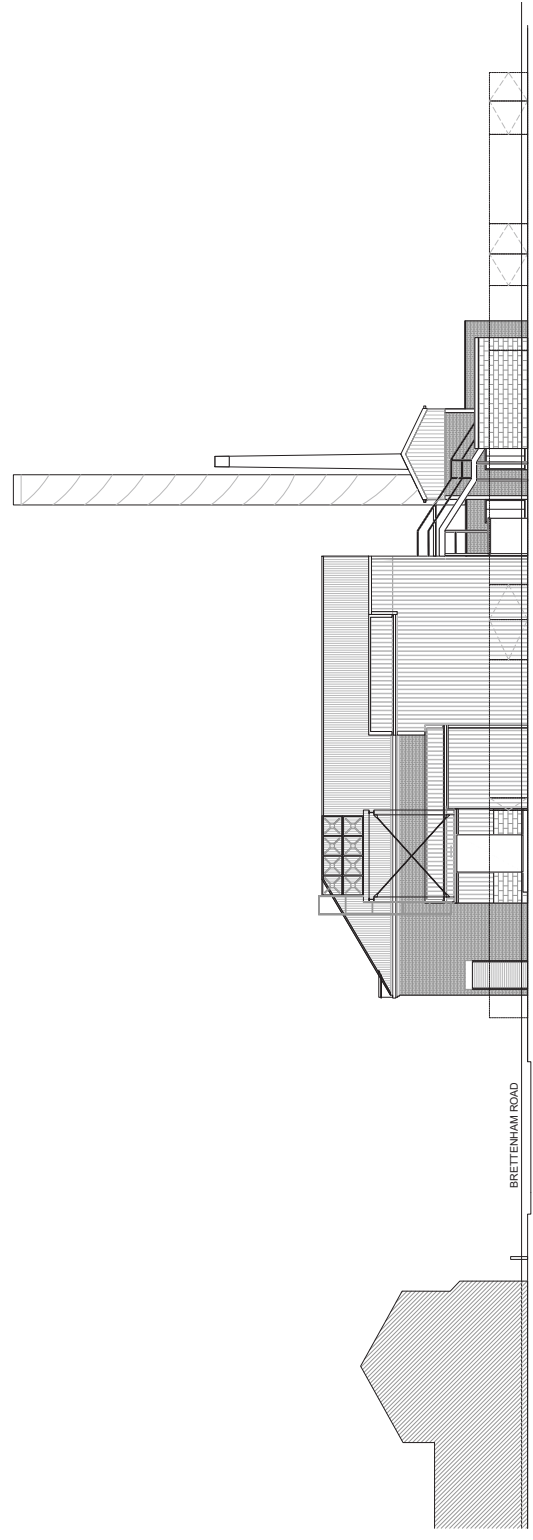
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EXISTING
ELEVATIONS

DATE: 03/12/16
DRAWN BY: RMW

SCALE: A3 @ 1:100 A4 @ 1:200

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BRETENHAM ROAD

2 EAST ELEVATION

Scale: 1:100 A0



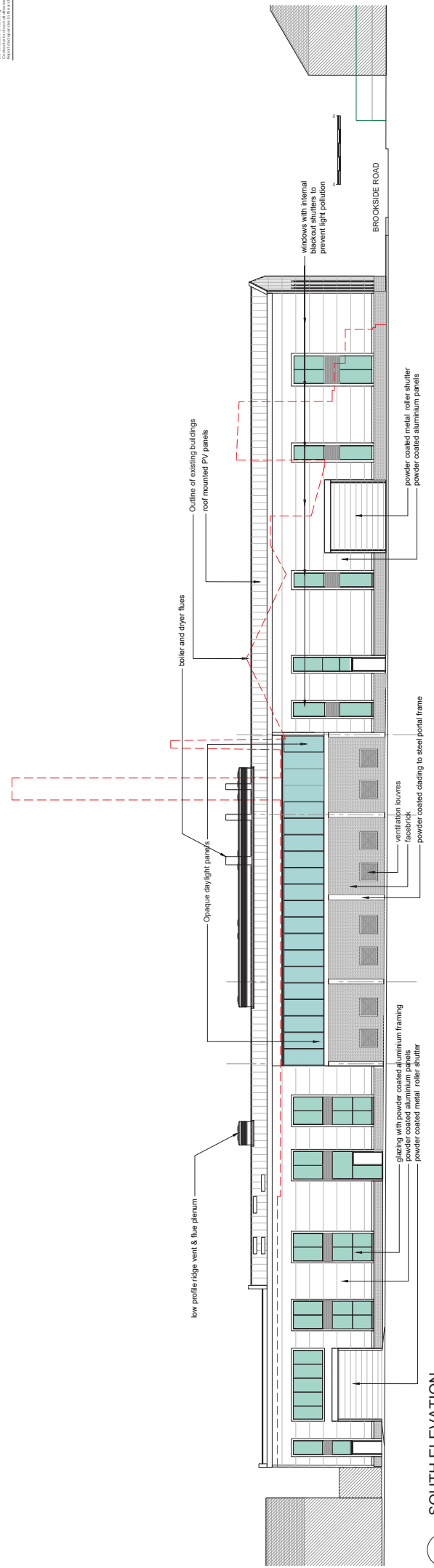
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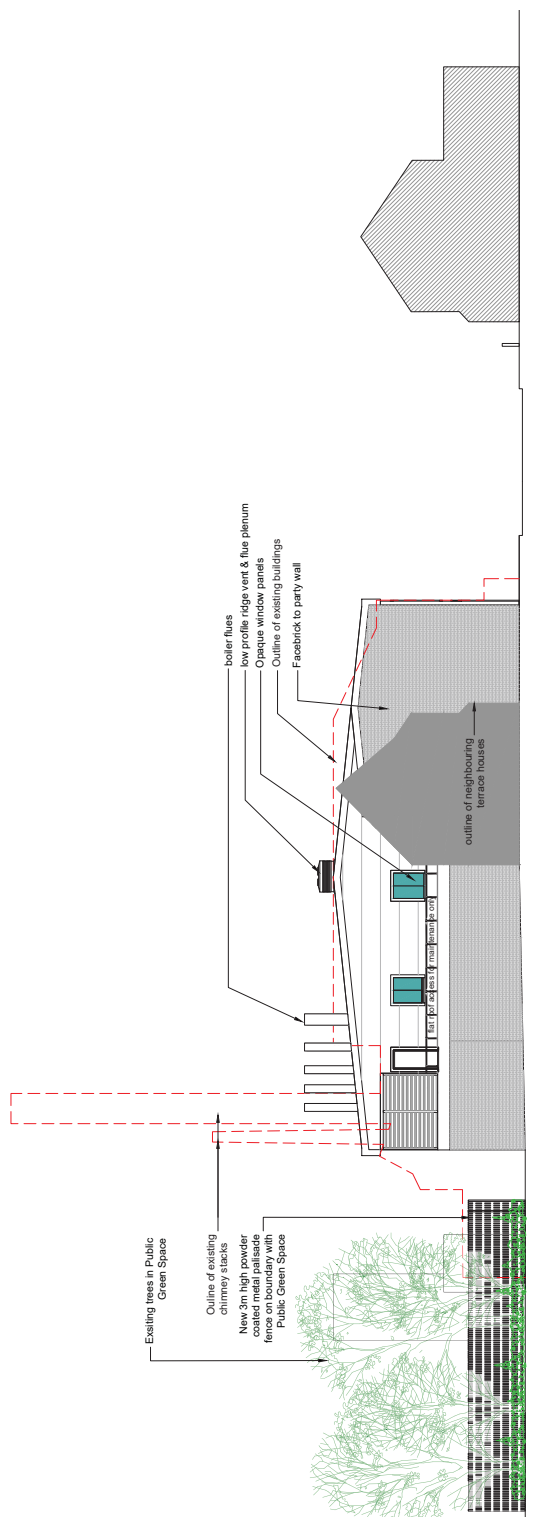
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 N18 2HE

PROPOSED
 ELEVATIONS

PLANNING
 DATE: 03.04.2018
 SCALE: 1:1000
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1 SOUTH ELEVATION
 Scale: 1:100 A0



2 WEST ELEVATION
 Scale: 1:100 A0

03	04.03.2018	Issued for Planning
02	21.08.2017	Demolish all
01	07.08.2018	Issued for Pre-App planning advice.
00	18.04.2018	Preliminary issue for review

rev	amendments	date	by
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BRETtenham STREET - SW VIEW



NW AERIAL VIEW



NE CORNER BROOKSIDE CLOSE AERIAL VIEW

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LONDON BOROUGH OF ENFIELD

PLANNING COMMITTEE

Date: 17th October 2019

Report of

Head of Planning

Contact Officer:

Andy Higham
 Claire Williams
 Eloise Kiernan
 Tel No: 020 8379 2180

Ward:

Winchmore Hill

Ref: 19/00201/FUL

Category: Full

LOCATION: 465-469 Green Lanes, London, N13 4BS

PROPOSAL: Redevelopment of the site to provide 15 residential units (including the re-provision of 1 existing 1 bed flat fronting Green Lanes) comprising 2 individual blocks, Block 1 - Three storey block of 3 flats comprising (1x1 bed, 1x2 bed and 1x3 bed) with balconies to first and second floor and Block 2 a Part 3, Part 4 storey block of 11 flats comprising 4x1 bed, 5x2 bed and 2x3 beds with balconies to front and rear, construction of a new access way off Green Lanes, off street parking, detached Bike/Bin store and associated landscaping.

Applicant Name & Address:

Andora Homes Ltd
 5 Corbar Close
 Enfield
 EN4 0JL
 United Kingdom

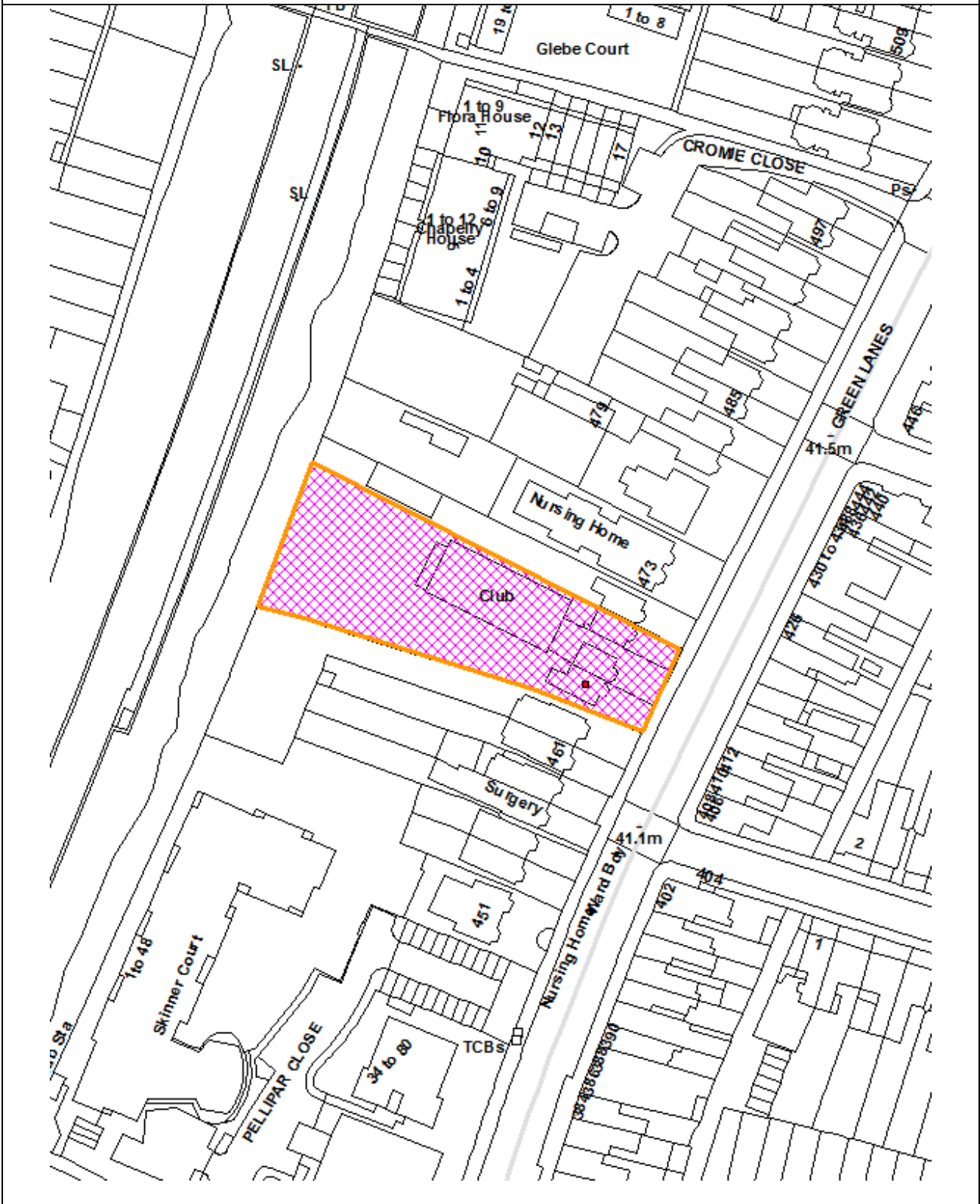
Agent Name & Address:

Mrs Carolyn Apcar
 Apcar Smith Planning
 Kinetic House
 Theobald Street
 Borehamwood
 Herts WD6 4PJ

Recommendation

That subject to the completion of a s106 agreement, the Head of Development Management / Planning Decisions Manager planning permission be **GRANTED** subject to conditions

Ref: 19/00201/FUL LOCATION: 465-469 Green Lanes, London, N13 4BS,



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North



1. Note for Members

- 1.1 Members will recall that this application was considered by the Planning Committee at its meeting on 27 August 2019. The application was deferred by Members in light of the concerns regarding the absence of any affordable housing contribution. Officers were asked to review the scheme viability further to establish whether there is scope for the development to support a contribution towards affordable housing
- 1.2 Further details regarding this matter are addressed below. This addendum should be read in conjunction with the original report prepared for the Planning Committee on 27 August 2019.
- 1.3 Following the Planning Committee on 29th August 2019, a second independent viability consultant, Dr Doug Birt was appointed to re-assess and review the submitted viability assessment.
- 1.4 The consultant has confirmed that the increased build costs for materials and decontamination of the site contained in the original reports are robust and an accurate reflection of current market conditions. In addition, a lower site value has been assumed for the redundant snooker halls than that taken into account when making the previous planning resolution. This corroborates with the finding by BNP Paribas and that of the submitted assessment by Bidwell's, as appointed by Andorra Homes.
- 1.5 On that basis, whilst acknowledging the concerns that exist regarding the loss of affordable housing, it is considered there is little alternative but to accept the conclusions of the viability assessments that have been undertaken. It remains the opinion of officers therefore that the submitted viability assessment is robust and that a financial contribution would make the scheme unviable. Given the need for housing across the Borough and the fact it is considered more housing could not be acceptable accommodated on this site, it remains the opinion of officers that the current scheme is acceptable.
- 1.6 This position must be viewed in the context of the current policy framework and in the context of guidance in the National Planning Policy Framework: that planning obligations should not threaten the deliverability or viability of a development.
- 1.7 It is therefore considered that the proposals remain acceptable and officer's recommendation planning permission should be granted subject to a S106 legal agreement and conditions
- 1.8 Whilst noting this position, However, following discussions with the applicants planning consultant, an off-site contribution of £63,970 could be provided towards affordable housing. This would comprise of the difference between the highway contributions required within the previous scheme ref. 15/05516/FUL and the current proposals, which equates to £28,480 plus the required off site educational contribution of £35,490, which could therefore be redirected towards affordable housing.
- 1.9 Mindful of the above and separate to the conclusion above and and officer recommendation to approve, it is noted that contributions have also been secured from the development towards highway improvements of £28,480 and educational of £35,490. If so minded and recognising the priority for affordable housing, the fact that highway improvements for cycling along Green Lanes are not now proceeding and that there are no education schemes in the immediate vicinity, a total of £63,970 could be redirected towards affordable housing.

2. Recommendation/Conditions

2.1 That subject to the completion of a S106 legal agreement, the Head of

Development Management/Planning Decisions Manager, be authorised to GRANT planning permission subject to the following conditions:

1. Time Limit

The development to which this permission relates must be begun not later than the expiration of three years beginning with the date of the decision notice.

Reason: To comply with the provisions of S.51 of the Planning & Compulsory Purchase Act 2004.

2. Approved Plans

The development hereby permitted shall be carried out in accordance with the approved plans, as set out in the attached schedule which forms part of this notice.

Reason: For the avoidance of doubt and in the interests of proper planning.

3. Materials

Prior to the commencement of development above ground, full details and materials of the external finishing to be used shall be submitted to and approved in writing by the Local Planning Authority. A schedule of materials and their use in the approved scheme is required and samples made available on site. A photograph showing all samples to be inspected must be submitted. The development shall be constructed in accordance with the approved details.

Reason: To ensure a satisfactory external appearance.

4. Contamination

Prior to commencement, a scheme to deal with the contamination of the site including an investigation and assessment of the extent of contamination and the measure to be taken to avoid risk to health and the environment has been submitted to and approved in writing by the Local Planning Authority.

Remediation shall be carried out in accordance with the approved scheme and the Local Planning Authority provided with a written warranty by the appointed specialist to confirm implementation prior to the occupation of development.

Reason: To avoid risk to public health and environment.

5. Contamination

If during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted and obtained written approval from the Local Planning Authority for, an amendment to the remediation strategy detailing how this unsuspected contamination shall be dealt with.

Reason: To protect against risks arising from contamination

6. Control of Dust and Emissions

All Non-Road Mobile Machinery (NRMM) of net power of 37kW and up to and including 560kW used during the course of the demolition, site preparation and construction phases shall comply with the emission standards set out in chapter 7 of the GLA's supplementary planning guidance "Control of Dust and Emissions During Construction and Demolition" dated July 2014 (SPG), or subsequent guidance. Unless it complies with the standards set out in the SPG, no NRMM shall be on site, at any time, whether in use or not, without the prior written consent of the local planning authority.

The developer shall keep an up to date list of all NRMM used during the demolition, site preparation and construction phases of the development on the online register at <https://nrmm.london/>

Reason: To protect local amenity and air quality in accordance with London Plan policies 5.3 and 7.14

7. Surfacing Materials

Prior to the commencement of development above ground, full details of the surfacing materials to be used within the development including footpaths, access roads and parking areas and road markings shall be submitted to and approved in writing by the Local Planning Authority. The surfacing shall be carried out in accordance with the approved detail before the development is occupied or use commences.

Reason: To ensure a satisfactory visual appearance and in the interests of highways safety.

8. Enclosure

The site shall be enclosed in accordance with details to be submitted to and approved in writing by the Local Planning Authority. The means of enclosure shall be erected in accordance with the approved detail before the development is occupied.

Reason: To ensure satisfactory appearance and safeguard the privacy, amenity and safety of adjoining occupiers and the public and in the interests of highway safety.

9. Landscaping

Prior to the commencement of development above ground, full details (including species, numbers and sizes) of trees, shrubs and grass to be planted on the site shall be submitted to and approved in writing by the Local Planning Authority. The planting scheme shall be carried out in accordance with the approved details in the first planting season after completion or occupation of the development whichever is the sooner. Any planting which dies, becomes severely damaged or diseased within five years of planting shall be replaced with new planting in accordance with the approved details. The landscaping scheme shall include the following landscaping and biodiversity enhancements:

- Planting plans;
- Written specifications (including cultivation and other operations associated with plant and grass establishment);
- Schedule of plants and trees, to include native and wildlife friendly species and large and large canopy trees in appropriate locations (noting species, planting sizes and proposed numbers / densities);
- Implementation timetables;
- Native and wildlife-friendly plants and trees of local or national provenance;

Reason: To provide a satisfactory appearance and to ensure the development provides the maximum possible provision towards the creation of habitats in accordance with Policies CP36 of the Core Strategy, DMD77, DMD80 and DMD81 of the Development Management Document and Policy 7.19 of the London Plan.

10. Biodiversity

Prior to the commencement of development, a full reptile survey and bat survey and an updated ecological report with details of biodiversity enhancements for the site shall be carried out by a suitably qualified ecologist. The recommendations are to be followed in full. A report detailing the results of the surveys, and the ecologist's recommendations, shall be submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with the approved details.

Reason: To minimize the impact of the development on the ecological value of the area and to ensure that the development provides the maximum possible provision towards the creation of habitats and valuable areas for biodiversity in accordance with policies CP36 of the Core Strategy, DMD76, DMD78 and DMD79 of the Development Management Document and 7.19 of the London Plan, as well as the advised outlined within the National Planning Policy Framework and the Biodiversity Action Plan.

11. Trees

Prior to the commencement of development above ground, an Arboricultural Impact Assessment (BS5837:2012) to fully consider the impacts on any existing trees, an arboricultural method statement and a tree protection plan shall be submitted to and approved in writing by the Local Planning Authority. The development shall be implemented in accordance with the approved details.

Reason: In the interests of visual amenities and to ensure that appropriate landscaping and tree species are retained, as well as ensuring that trees are protected during development, having regard to policies DMD80 and DMD81 of the Development Management Document.

12. Refuse Storage & Recycling Facilities

The development excluding demolition and groundwork shall not commence until details of refuse storage facilities including facilities for the recycling of waste to be provided within the development, in accordance with the London Borough of Enfield - Waste and Recycling Planning Storage Guidance ENV 08/162, have been submitted to and approved in writing by the Local Planning Authority. The facilities shall be provided in accordance with the approved details before the development is occupied or use commences.

Reason: In the interests of amenity and the recycling of waste materials in support of the Boroughs waste reduction targets.

13. Cycle parking spaces

The development excluding demolition and groundwork shall not commence until details of the siting, number and design of secure/covered cycle parking spaces (26 long stay and 1 short stay) have been submitted to and approved in writing by the Local Planning Authority. This shall include details of cycle storage where possible within the private garden areas on the ground floor in addition to an additional cycle parking storage to the front communal area. The approved details shall thereafter be installed and permanently retained for cycle parking.

Reason: To ensure the provision of cycle parking spaces in line with the Council's adopted standards.

14. Obscured Glazing

The glazing to be installed on the side elevation of Block B shall be in obscured glass with an equivalent obscuration as level 3 on the Pilkington Obscuration Range and fixed to a height of 1.7 metres above the floor level of the room to which they relate. The glazing shall not be altered without the approval in writing of the Local Planning Authority.

Reason: To safeguard the privacy of the occupiers of adjoining properties.

15. Access

The development excluding demolition and ground work shall not commence until full details of the proposed undercroft access including points a – d are submitted to and approved in writing by the Local Planning Authority:

- a. Details of the access ramps;
- b. The treatment to the front driveway area of Number 469 including details of retaining walls and boundary treatments to 467 and 471 Green Lanes;

- c. Details of a Priority/ Waiting arrangement to allow for safe two-way vehicle movement, prioritising vehicles entering the site; and
- d. Details of the Pedestrian Access path and its separation from the undercroft vehicular access.

These works shall be carried out in accordance with the approved details before development is occupied or the use commences.

Reason: To ensure that the development complies with Council Policies and does not prejudice conditions of safety or traffic flow on adjoining highways.

16. Construction Methodology

That development shall not commence until a construction methodology has been submitted to and approved in writing by the Local Planning Authority. The construction methodology shall contain:

- a. arrangements for wheel cleaning;
- b. arrangements for the storage of materials;
- c. hours of work;
- d. arrangements for the securing of the site during construction;
- e. the arrangement for the parking of contractors' vehicles clear of the highway;
- f. The siting and design of any ancillary structures; and
- g. A construction management plan written in accordance with the 'Mayor of London's supplementary planning guidance 'The Control of Dust and Emissions During Construction and Demolition' detailing how dust and emissions will be managed during demolition and construction work.

The development shall be carried out in accordance with the approved construction methodology unless otherwise agreed in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not lead to damage to the existing highway and to minimise disruption to neighbouring properties and the environment.

17. External Lighting

The development, excluding groundwork and demolition shall not commence until details of any external lighting proposed have been submitted to and approved in writing by the Local Planning Authority. The approved external lighting shall be provided before the development is occupied.

Reason: To ensure that the development does not prejudice the amenities of adjoining occupiers and / or the visual amenities of the surrounding area.

18. Energy Statement

Notwithstanding the submitted energy statement, prior to the commencement of above ground works, an updated energy statement shall be submitted to and approved in writing by the Local Planning Authority. The development shall provide for no less than a 35% reduction on the total CO2 emissions

arising from the operation of a development and its services over Part L of Building Regs 2013.

The location and details of renewable technology to be installed along with the maintenance and management strategy for their continued operation shall be submitted to and approved by the Local Planning Authority.

The development shall be carried out strictly in accordance with the energy statement so approved and maintained as such thereafter.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan and the NPPF.

19. EPC's

Following practical completion of works a final Energy Performance Certificate shall be submitted to and approved in writing by the Local Planning Authority prior to occupation of the development.

Reason: In the interest of sustainable development and to ensure that the Local Planning Authority may be satisfied that CO2 emission reduction targets are met in accordance with Policy CP20 of the Core Strategy, Policies 5.2, 5.3, 5.7 & 5.9 of the London Plan 2011 and the NPPF.

20. SuDS

The development shall not commence until a Sustainable Drainage Strategy has been submitted to and approved in writing by the Local Planning Authority. The details shall be based on the disposal of surface water by means of a sustainable drainage system in accordance with the principles as set out in the Technical Guidance to the National Planning Policy Framework and should be in line with our DMD Policy SuDS Requirements:

- a. Shall be designed to a 1 in 1 and 1 in 100 year storm event with the allowance for climate change;
- b. Follow the SuDS management train and London Plan Drainage Hierarchy by providing a number of treatment phases corresponding to their pollution potential;
- c. Should maximise opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value;
- d. The system must be designed to allow for flows that exceed the design capacity to be stored on site or conveyed off-site with minimum impact;
- e. Clear ownership, management and maintenance arrangements must be established; and
- f. The details submitted shall include levels, sizing, cross sections and specifications for all drainage features.

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF and to maximise

opportunities for sustainable development, improve water quality, biodiversity, local amenity and recreation value.

21. SuDS

Prior to occupation of the development, a Verification Report demonstrating that the approved drainage / SuDS measures have been fully implemented shall be submitted to the Local Planning Authority for approval in writing. This report must include:

- a. As built drawings of the sustainable drainage systems;
- b. Level surveys of completed works;
- c. Photographs of the completed sustainable drainage systems;
- d. Any relevant certificates from manufacturers/ suppliers of any drainage features;
- e. A confirmation statement of the above signed by a chartered engineer.

Reason: To ensure the sustainable management of water, minimise flood risk, minimise discharge of surface water outside of the curtilage of the property and ensure that the drainage system will remain functional throughout the lifetime of the development in accordance with Policy CP28 of the Core Strategy and Policies 5.12 & 5.13 of the London Plan and the NPPF.

22. Electric Parking Bays

The development shall not be occupied until details confirming that i) three electric parking bays will be provided, and ii) all remaining bays (nine) will be passive electric bays. The development shall be carried out in accordance with the approved details and retain in perpetuity unless otherwise agreed with the Local Planning Authority.

Reason: To promote sustainable transport measures and to ensure that the provision is in accordance with London Plan standards.

23. Green Roofs

Prior to the commencement of the superstructure details of a biodiversity (green/brown) roof(s) to be installed shall be submitted to and approved in writing by the Local Planning Authority. The biodiversity (green/brown) roof(s) shall be:

- a. Biodiversity based with extensive substrate base (depth 80-150mm);
- b. Planted/seeded with an agreed mix of species within the first planting season following practical completion of the building works.

The biodiversity (green/brown) roof shall not be used for any recreational purpose and access shall only be for the purposes of the maintenance and repair or means of emergency escape. Details shall include full ongoing management plan and maintenance strategy/schedule for the green/brown roof to be approved in writing by the Local Planning Authority.

The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To minimise the impact of the development on the ecological value of the area and to ensure the development provides the maximum possible

provision towards the creation of habitats and valuable areas for biodiversity in accordance with Policy CP36 of the Core Strategy, the Biodiversity Action Plan and Policies 5.11 & 7.19 of the London Plan.

24. Site Waste Management Plan

Notwithstanding the approved documents, the development shall not commence until a revised Site Waste Management Plan has been submitted to and approved in writing by the Local Planning Authority. The plan should include as a minimum:

- i. Target benchmarks for resource efficiency set in accordance with best practice
- ii. Procedures and commitments to minimize non-hazardous construction waste at design stage. Specify waste minimisation actions relating to at least 3 waste groups and support them by appropriate monitoring of waste.
- iii. Procedures for minimising hazardous waste
- iv. Monitoring, measuring and reporting of hazardous and non-hazardous site waste production according to the defined waste groups (according to the waste streams generated by the scope of the works)
- v. Procedures and commitments to sort and divert waste from landfill in accordance with the waste hierarchy (reduce; reuse; recycle; recover) according to the defined waste groups

In addition, no less than 85% by weight or by volume of non-hazardous construction, excavation and demolition waste generated by the development has been diverted from landfill

Reason: To maximise the amount of waste diverted from landfill consistent with the waste hierarchy and strategic targets set by Policy DMD57 of the Development Management Document and Policies 5.17, 5.18, 5.19, 5.20 of the London Plan.

25. Rainwater Recycling System

The development shall not commence until details of a rainwater recycling system has been submitted to and approved in writing by the Local Planning Authority. The details submitted shall also demonstrate the maximum level of recycled water that can feasibly be provided to the development. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To promote water conservation and efficiency measures in all new developments and where possible in the retrofitting of existing stock in accordance with Policy CP21 of the Core Strategy, Policies DMD58 and DMD61 of the Development Management Document and Policy 5.15 of the London Plan.

26. Clearance of vegetation during bird nesting

No areas of hedges, scrub or similar vegetation where birds may nest shall be cleared outside of the bird nesting season (March-August inclusive). Should clearance during the bird-nesting season be unavoidable, a suitably qualified ecologist shall assess the areas to be removed prior to clearance, and if any active nests are recorded then no further works shall take place until all young have fledged the nest.

Reason: To ensure that wildlife is not adversely impacted by the development, in accordance with policy CP36 of the Core Strategy and the National Planning Policy Framework

27. Considerate Constructors

The development shall not commence until an undertaking to meet with best practice under the Considerate Constructors Scheme and achieve formal certification has been submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure the implementation of the development does not adversely impact on the surrounding area and to minimise disruption to neighbouring properties.

28. Water Efficiency

Prior to occupation details of the internal consumption of potable water shall be submitted to and approved in writing by the Local Planning Authority. Submitted details will demonstrate reduced water consumption through the use of water efficient fittings, appliances and recycling systems to show consumption equal to or less than 105 litres per person per day as stated in the pre-assessment accompanying the scheme. The development shall be carried out strictly in accordance with the details so approved and maintained as such thereafter.

Reason: To promote water conservation and efficiency measures in all new developments and where possible in the retrofitting of existing stock in accordance with Policy CP21 of the Core Strategy, DMD58 of the Development Management Document and Policy 5.15 of the London Plan.

3. Executive Summary

- 3.1 This application is identical to planning application reference number 15/05516/FUL. The Planning Committee resolved to grant planning permission subject to the completion of a S106 legal agreement and conditions on 27 June 2017. The decision was issued on 14 August 2018.
- 3.2 There have been no material changes on the application site and all other aspects of the development remain as per the previously submitted scheme. The key difference is the updated viability assessment, which confirms that an onsite or off-site contribution towards affordable housing would make the proposed development unviable. Officers consider it necessary to secure two review mechanisms through a S106 agreement to capture future affordable housing on the site, having regard to the Major of London's Affordable Housing and Viability SPD. The S106 would also capture a financial contribution towards healthy streets and sustainable transport and education, as well as a business, employment and skills strategy.
- 3.3 The proposed development is of an acceptable design to integrate satisfactorily within the site and surrounding area. It would contribute an additional 14 units to the Boroughs housing stock, including 3 additional family units within a relatively accessible part of the Borough.

- 3.4 Having due regard to the updated National Planning Policy Framework, officers have fully reviewed the proposal having regard to biodiversity, landscaping and sustainability and attached appropriate conditions to achieve a highly sustainable form of development within the Borough in line with planning policy requirements.

4. Site and Surroundings

- 4.1 The application site is 465-469 Green Lanes, London, N13 4BS. The site is located on the western side of Green Lanes a short distance south of the crossroads junction with Hedge Lane and Bourne Hill. The site is predominantly flat lying, rectangular in shape though widens at the rear towards the rail tracks boundary. At present to the front the site consists of three Victorian style properties 465 and 467, a pair of semi-detached properties and 469 which forms the next pair of semi-detached properties with Number 471 Green Lanes, this does not form part of the application site. Number 465-469 appear to be laid out each as 2 residential flats over ground and first floor level. This has been confirmed by lawful development certificates in recent years individually for each property.
- 4.2 To the rear of 465-469 sits a single storey warehouse style building with a part mezzanine level. From inspections on site this building is dilapidated with the roof having fallen in. The recent planning history on this building show historically it was a snooker club however more recently it has been used as a Private social club and casino but has been closed down in recent years as a result of security and licensing issues. This building was accessed between Number 467 and 469.
- 4.3 The surrounding area is mixed in nature. This side of Green Lanes is predominantly residential, made up of original houses or properties that are converted into flats. There is an office building next door at Number 471 and the Conifers Nursing home is further north on Green Lanes. To the west the site is bound by the railway line and to the east and west are extensive deep gardens. There is a bungalow towards the end of the rear garden of Number 471 flanking the site to the north.
- 4.4 The site is not located in a Conservation Area and is not listed. The site has a PTAL rating of 3. The site is not located within a controlled parking zone and it is relatively flat lying. It has a total site area of approximately 2500 sqm or 0.25 hectares. The site is located within a wildlife corridor.

5. Proposal

- 5.1 The applicant seeks full planning permission for the re-development of the site to provide 15 residential units (including the re-provision of 1 existing 1 bed flat fronting Green Lanes) comprising 2 individual blocks, Block 1 -Three storey block of 3 flats comprising (1x1 bed, 1x2 bed and 1x3 bed) with balconies to first and second floor and Block 2 a Part 3, Part 4 storey block of 11 flats comprising 4x1 bed, 5x2 bed and 2x3 beds with balconies to front and rear, construction of a new access way off Green Lanes, off street parking, detached Bike/Bin store and associated landscaping.
- 5.2 For access reasons this will involve the demolition of Number 469 for the creation of a vehicular and pedestrian access to the two blocks at the rear.

- 5.3 Behind the front building line two separate residential blocks of flats are proposed. Block 1 is the smaller block towards the front of the site. This would be mainly 2 storeys high with a recessed third floor level and would accommodate 3 flats (1x1 bed, 1x2 bed and 1x3 bed). This block would be 17m wide with a stepped depth and it would be 8.5m high. It would be set 10m from the rear garden boundary of the original property at Number 465 and a distance of approximately 21 metres from the original rear wall of Number 465. The building would be set 15m obliquely from the rear elevation of Number 463 to the south.
- 5.4 The second larger block, Block B would be set to the western end of the site closer to the railway line. This block would be part 3 to part 4 storeys in height. It would be 23 metres in width with a stepped front and rear elevation with an average depth of approximately 15 metres. It would be set on average 1.5m from the southern end of the site and 7.5m from the northern end of the site and an average of 5m from the rear western end of the site that flanks the railway line. The building would have a height of 9m for the 3 storey section with the raised parapet and 11.5m to the top of the 4th floor. The building is to be proposed in a mixture of buff brick, render, glazed balconies and aluminium materials.
- 5.5 In addition to the new proposed access under the undercroft of Number 469 12 car parking spaces are proposed inclusive of 1 disabled space. The remainder of the site would consist of the access road, private and communal garden spaces and landscaped areas in front of both blocks. There would be a single storey building in the northwest corner that would operate provide 26 cycle parking spaces and a refuse store for 4x1100l bins.
- 5.6 Planning Committee members resolved to grant the original planning permission (reference no. 15/05516/FUL) subject to conditions and a S106 to secure an offsite contribution towards affordable housing, education and local highway works and appropriate conditions on 27 June 2017. The site has since been sold on to Andorra Homes and the resubmission is identical to the approved scheme with the exception of any off site or on site affordable housing provision, and as such the key consideration thereby relates to a reassessment of viability matters, as well as any further considerations pertaining to appropriate contributions in light of the recently revised National Planning Policy Framework (2019).

6. Relevant planning history

- 6.1 15/05516/FUL - Redevelopment of the site to provide 15 residential units (including the re-provision of 1 existing 1 bed flat fronting Green Lanes) comprising 2 individual blocks, Block 1 -Three storey block of 3 flats comprising (1x1 bed, 1x2 bed and 1x3 bed) with balconies to first and second floor and Block 2 a Part 3, Part 4 storey block of 11 flats comprising 4x1 bed, 5x2 bed and 2x3 beds with balconies to front and rear, construction of a new access way off Green Lanes, off street parking, detached Bike/Bin store and associated landscaping – granted subject to an S106 to secure affordable housing, education and highway improvements and appropriate conditions on 14 August 2018.
- 6.2 P1200069PLA: Change of use from D2 snooker hall to social club /function hall sui generis (RETROSPECTIVE) -Refused and Dismissed at Appeal.

- 6.3 15/00247/CEU: 465 Green Lanes. Use of premises as two self-contained flats.
- Granted 18.03.2015.
- 6.4 15/00248/CEU: 467 Green Lanes. Use of premises as two self-contained flats
- Granted 18.03.2015.
- 6.5 15/00249/CEU: 469 Green Lanes. Use of premises as two self-contained flats
- Granted 20.05.2015.

7. Consultation

7.1 Statutory and non-statutory consultees

Internal

- 7.2 Traffic and Transportation – No objection subject to conditions and a s106 agreement securing financial contribution towards highway works.
- 7.3 Environmental Health - No objection subject to conditions.
- 7.4 Housing - A minimum of 6 units should be provided towards affordable housing, 4 as social or affordable rent and 2 as intermediate.
- 7.5 SuDS –No objection subject to the submission of further information through a condition.
- 7.6 Tree officer –No objection raised subject to additional information.

External

- 7.7 Thames Water - No objection.
- 7.8 Environment Agency - No objection.

Public

- 7.9 The 21 day public consultation period started on the 19th February 2019 and concluded on the 12th March 2019. Site notices were posted close to the site on 5 March 2019. The application was also advertised in the local paper. There were no comments received from any members of the public.

8. Relevant Planning Policies

8.1 Development Management Document

- DMD1 Affordable Housing on site capable of providing 10 or more units.
- DMD3 Providing a Mix of Different Sized Homes
- DMD6 Residential Character
- DMD7 Development of garden land
- DMD8 General Standards for New Residential Development
- DMD9 Amenity Space
- DMD10 Distancing
- DMD37 Achieving High Quality and Design-Led Development
- DMD47 New Roads, Access and Servicing
- DMD49 Sustainable Design and Construction Statements

DMD50	Environmental Assessment Methods
DMD51	Energy Efficiency Standards
DMD60	Assessing Flood Risk
DMD61	Managing Surface Water
DMD64	Pollution Control and Assessment
DMD68	Noise
DMD69	Light Pollution
DMD76	Wildlife Corridors
DMD77	Green Chains
DMD78	Nature Conservation

8.2 Core Strategy

SO2	Environmental sustainability
SO4	New homes
SO5	Education, health and wellbeing
SO8	Transportation and accessibility
SO10	Built environment
CP2	Housing supply and locations for new homes
CP3	Affordable housing
CP4	Housing quality
CP5	Housing types
CP6	Meeting particular housing needs
CP8	Education
CP9	Supporting community cohesion
CP16	Taking part in economic success and improving skills
CP20	Sustainable energy use and energy infrastructure
CP21	Delivering sustainable water supply, drainage and sewerage infrastructure
CP22	Delivering sustainable waste management
CP24	The road network
CP25	Pedestrians and cyclists
CP28	Managing flood risk through development
CP30	Maintaining and improving the quality of the built and open environment
CP32:	Pollution
CP36	Biodiversity
CP46	Infrastructure Contribution

8.3 London Plan (2016)

3.3	Increasing housing supply
3.4	Optimising housing potential
3.5	Quality and design of housing development
3.6	Children and young people's play and informal recreation facilities
3.8	Housing choice
3.9	Mixed and balanced communities
3.10	Definition of affordable housing
3.11	Affordable housing targets
3.12	Negotiating affordable housing on schemes
3.13	Affordable housing thresholds
4.1	Developing London's economy
4.4	Managing industrial land and premises
5.1	Climate change mitigation
5.2	Minimising carbon dioxide emissions
5.3	Sustainable design and construction
5.7	Renewable energy

- 5.8 Innovative energy technologies
- 5.10 Urban greening
- 5.11 Green roofs and development site environs
- 5.13 Sustainable drainage
- 5.14 Water quality and wastewater infrastructure
- 5.15 Water use and supplies
- 5.16 Waste self sufficiency
- 6.3 Assessing the effects of development on transport capacity
- 6.9 Cycling
- 6.12 Road network capacity
- 6.13 Parking
- 7.1 Building London's neighbours and communities
- 7.2 An inclusive environment
- 7.3 Designing out crime
- 7.4 Local character
- 7.5 Public realm
- 7.6 Architecture
- 7.19 Biodiversity and access to nature
- 7.21 Trees and Woodland

8.4 Other Relevant Policy

- National Planning Policy Framework (2019)
- National Planning Practice Guidance (2019)

8.5 Other Material Considerations

- The Mayors Housing Supplementary Planning Guidance (2016)
- Section 106 Supplementary Planning Document (SPD) (2016)
- Enfield Strategic Housing Market Assessment (2015)
- Nationally Described Space Standards
- Mayor of London's Affordable Housing and Viability Supplementary Planning Document (SPD) (2017)
- The Draft London Plan (published on 29 November 2017)

9. **Analysis**

9.1 The main issues for consideration regarding this application are as follows:

- Principle of the Development;
- Scale and Density;
- Design and Impact on the Character of the Surrounding Area;
- Neighbouring Residential Amenity;
- Standard of Accommodation and Proposed Mix of Units;
- Private Amenity provisions;
- Traffic, Parking and Servicing Issues;
- Contamination;
- Affordable Housing and other S106 Contributions;
- Sustainable Design and Construction;
- Trees and Biodiversity; and
- Sustainable Drainage Systems (SuDS).

9.2 Principle of the Development

- 9.2.1 Policy DMD 7 states that the Council seeks to protect and enhance the positive contribution gardens make to the character of the Borough. Development on garden land will only be permitted if all of the following criteria are met:
- a. The development does not harm the character of the area
 - b. Increased density is appropriate taking into account the site context in terms of its location, accessibility and the provision of local infrastructure;
 - c. The original plot is of a sufficient size to allow for additional dwellings which meet the standards in DMD 8 'General Standards for New Residential Development', (and other design policies);
 - d. The individual plot sizes, orientation and layout created are appropriate to, and would not adversely impact on the residential amenity within the development, or the existing pattern of development in that locality;
 - e. An adequate amount of garden space is retained within both of the individual plots in accordance with the minimum amenity space standards (DMD 9 'Amenity Space'), and the role of each space is enhanced to contribute towards other plan objectives such as biodiversity; green corridors and networks; flood risk; climate change; local context and character; and play space
 - f. The proposals would provide appropriate access to the public highway.
- 9.2.2 The proposal involves development within a backland location. Policy DMD7 highlights the importance that gardens make to the contribution of the character of the borough. However, in this instance the site has previously been developed and the dilapidated building remains in place. In addition, at approximately 2500 sqm it is a substantial site that is capable of accommodating development. The gardens of properties to the north have been developed in various ways, including the existing bungalow, as such the proposal would not disturb the character and appearance. No. 469 Green Lanes currently serves as 2 x 1-bed flats; however one flat would be replaced as well as a net gain of 14 residential units, including four family units within a relatively accessible location within Palmers Green. On that basis, the principle of development is therefore considered acceptable subject to further considerations as outlined below.
- 9.2.3 Additionally, it was noted that this is the resubmission of an identical scheme ref. 15/05516/FUL, which the Planning Committee resolved to grant planning permission in June 2017. The previously granted scheme thus is a material consideration with appropriate weight, however additional regard must be given to any other relevant matters such as any material change in policy direction during this time, which includes the revised National Planning Policy Framework (2019).

9.3 Density and Scale

Density

- 9.3.1 Density assessments must acknowledge new guidance outlined in the NPPF and particularly the London Plan, which encourage greater flexibility in the

application of policies to promote higher densities, although they must also be appropriate for the area.

9.3.2 Policy 3.4 (Table 3.2) of the London Plan sets standards for appropriate density levels with regards to location, existing building form, massing, and having regard to the PTAL (Public Transport Accessibility Level) score. From assessment of the plans, it is considered a total of 40 habitable rooms would be provided on the site. In addition to this there would be 5 existing 1 bed flat in the original properties to the front retained which would include for an additional 10 habitable rooms. The site area which is of 0.25 hectares. According to the guidance in (Table 3.2) of the London Plan as the site has a site specific PTAL rating of 3 in a suburban location, an overall density of between 150-250 hr/ha and 35-65u/ha may be acceptable. Upon calculating the density of the proposed development against this density matrix, based on habitable rooms per hectare this development would equate to 200 hr/ha and 56u/ha, which fall within the specified range.

9.3.3 Therefore these results show that from a density perspective this proposal would be mid-range and thereby within a recognisable density threshold for the area.

9.3.4 However, density should be considered alongside other planning requirements such as suitability of the site, scale of building/s and standard and quality of accommodation proposed. In this case due to the tightness of the site neighbouring amenity would also be a primary consideration.

9.4 Scale, Design, Character and Impact on the Surroundings

9.4.1 The application proposes two blocks, Block 1 being a part 2, part 3 storey building of 3 flats and Block 2 a part 3, part 4 storey building to accommodate 11 flats.

9.4.2 The previous scheme involved several revisions to improve the overall design in regard to the proposed bulk, scale and prominence of the fourth floor, the original large hipped roof and the fact the original application sought to leave a gap in the front Green Lanes street scene.

9.4.3 These matters were addressed and have been retained within the design of the current scheme. This scheme does not seek to make any changes to the design of the scheme. The scheme proposes a contemporary design concept with a recessed cladded 3rd and 4th floor levels to reduce bulk and height of the buildings within the street scene at Green Lanes. Additionally, the overall design to no. 469 incorporates the original Victorian façade including hipped roofline to integrate satisfactorily and provide a frontage within the established street scene.

9.4.4 Additionally, the two rear blocks again feature a contemporary appearance of an acceptable scale, bulk and massing. It is therefore concluded that from a design perspective, the buildings are appropriately designed to fit into the context of the site. Additionally, the contrast in materials between brick, render, the metal cladding, glazed balconies and aluminium would allow for

both blocks to have an acceptable appearance and create an element of visual interest.

- 9.4.5 From the perspective of scale it is considered that Block 1 is comparable in scale to the other buildings in the area. Block B to the rear of the site is a large building for a backland location standing at 4 storeys in height. However due regard is given to the site's expansive depth and width, the scale of neighbouring rear gardens and existing back land development. As outlined below it is considered that the site could accommodate the development without detrimentally impacting upon neighbouring amenities. The building is located at the furthest most end of the site and is reasonably well set in from neighbouring common boundaries and set at a distance in excess of 45 metres from the rear elevation of Number 463 and in excess of 50m from the rear elevation of Number 463.
- 9.4.6 In addition from the perspective of the front street scene it is considered that both proposed blocks would have a relatively limited impact on the Green Lanes street scene, due to the fact that the first-floor level of no. 469 is being re-provided and as such both blocks would be essentially screened from view within the front street scene.
- 9.4.7 In conclusion, the proposed design, scale and character are considered acceptable as it would integrate acceptably into the adjoining locality and the Green Lanes street scene having regard to policies DMD6, 8 and 37, CP30 of the Core Strategy and London Plan policies 7.4 and 7.6.

9.5 Neighbouring Amenity

- 9.5.1 From the perspective of neighbouring amenity, it is considered that the key properties impacted on by the development would be the original properties at no's 463-469 Green Lanes and no. 471a Green Lanes (bungalow in rear garden of no. 471).
- 9.5.2 Policy 7.6 of the London Plan states that developments should have appropriate regard to their surroundings, and that they improve the environment in terms of residential amenity. Policy CP30 of the Enfield Core Strategy seeks to ensure that new developments are high quality and designed, having regards to their context. They should help to deliver Core Strategy policy CP9 in supporting community cohesion by promoting attractive, safe, accessible and inclusive neighbourhoods. Policy DMD8 states that new developments should preserve amenity in terms of daylight, sunlight, outlook, privacy, overlooking, noise and disturbance.

Original Properties 463-469 Green Lanes

- 9.5.3 Whilst 465-469 are within the applicant's ownership the impact on the amenity of future residents remains a consideration. From the back of Block 1 there is a distance of 10m to what will be formed as a new rear garden boundary serving Number 465 and 467. In addition to this the distance between rear facing windows is approximately 21m and overall it is considered there is adequate distance to ensure upon sufficient privacy between facing windows.

Officers are satisfied that it has an acceptable relationship with these properties.

9.5.4 Number 463 adjacent to the application site has a slightly deeper outrigger resulting in a separation distance of approximately 16 metres between Block 1 and Number 463. However, this would be set obliquely at an angle to Block 1 and it is considered there would be sufficient privacy retained. Block 1 would be set at distances of 1.2m and 3m away from the rear garden boundary of Number 463. Whilst this would normally represent an imposing structure in most instances due to the expansive depths and width of the gardens that are backing on from these properties there is a considerable sense of space and openness, having examined the relationship on site and the separation of Block 1 from the rear of Number 463, it is considered that there would not be an unacceptable impact on the amenity of the occupiers having regard to policies DMD6, DMD8 and DMD10 of the DMD. There are no side facing windows on Block 1 to create unacceptable overlooking onto the rear garden of no. 463.

9.5.5 At the western furthest end of the site Block 2 is set stepped at an average distance of 1.5m from the rear garden boundary of no. 463 Green Lanes and at a distance in excess of 40m from the rear facing wall of no. 463. As referred to earlier in the report regard is given to the fact that at 4 storeys in height within this backland location, this building is relatively large. However due to the sense of space, depth, width and relationship between the plots it is considered this building can be acceptably accommodated on this site. Having assessed this proposal on site and given the fact that it is close to the rear garden boundaries, at the distance in excess of 40m away from no. 463 it is considered that enough of space and distancing is created so Block B would not unacceptably overlook or become too overbearing onto the rear garden of no. 463. At present similar to many properties on this section of Green Lanes, no. 463 appears to be split in flats. The garden to the rear is substantial and stretches down towards the railway lines alongside no. 465 and then doglegs to the left towards Skinners Court to the south. It is a very substantial garden area however is relatively unkempt, overgrown and it appears that only the immediate patio area to the rear of no. 463 is used. Whilst this would not be a determining factor, officers have analysed the application thoroughly on site and overall the impact onto no. 463 and the expansive rear garden is considered acceptable on balance.

471a Green Lanes - Bungalow in rear garden of Number 471

9.5.6 No. 471a is unusual in its setting in that it is a standalone bungalow house set to the rear of no. 471 Green Lanes. It appears to be accessed via the side of Green Lanes and from examinations on site appears to be in residential use. There does not appear to be any planning history on record but from checking mapping and aerial records it appears to have been in place for a number of years and for the purposes of this application it has been regarded as a lawful structure. This single storey bungalow is sandwiched in between the application site and the Conifers nursing home to the north and is positioned towards the lower end of the site approximately 30m down the garden. From

examinations on site the occupants appear to use the area to the front and rear of the bungalow as amenity space.

- 9.5.7 Due to the separation distance, Block 1 would have a limited and acceptable impact.
- 9.5.8 Block 2 would be set 7.5m from the side boundary with no. 471a Green Lanes at part three, part four stories in height. Officers have examined this relationship on site and it is considered the impact is acceptable. There are no side facing windows in Block 2 that would create privacy issues to the south facing windows on no. 471a and all windows in Block 2 with the exception of one ground level hall window are facing east-west 90 degrees away from no. 471a. Block 2 would be more visually prominent in terms of outlook from these ground level windows, however at the moment the outlook from these windows is onto a high boundary line and therefore poor at present. Whilst Block 2 would be obviously visible from these windows there is a considerable gap between Blocks 1 and 2 and in addition to the distance that Block 2 is set away from the boundary overall officers are satisfied there is an acceptable impact in terms of outlook onto the occupiers of no. 471a. Similarly, whilst Block 2 in particular would be visibly noticeable from the rear and front garden areas of no. 471a, it is considered it is adequately positioned away from the boundary line of this property to not appear too visually imposing. Regarding daylight and sunlight whilst Block B has the potential to block some sunlight from these south facing windows it would only be for a relatively short period in the late afternoon. The proposed buildings are set far enough away and there is sufficient gap between Blocks 1 and 2 and to the rear of Block 2 to allow enough direct light into no. 471 Green Lanes.
- 9.5.9 The communal bin and cycle store is proposed to the rear of no. 471a. However, there is a slight drop in ground levels of about 300mm at this section of the site with no. 471a sitting at a higher land level. As a result, the eaves level of this store would be approximately 2m on the boundary line with a low level hipped roof that slopes away. Having examined this relationship on site it is considered there is an acceptable impact from this structure onto the amenities of the occupiers of no. 471a.
- 9.5.10 In conclusion all factors considered the proposal has an acceptable impact in terms of neighbouring amenity to all adjoining occupiers.

9.6 Standard of Accommodation and Proposed Mix of Units

Standard of Accommodation

- 9.6.1 Policy 3.5 of the London Plan 2016 and Policies DMD 5 and DMD 8 of the Development Management Document (2014) set minimum internal space standards for residential development. The Nationally Described Internal Space Standard applies to all residential developments within the Borough and the London Plan Housing SPG adopted in 2016 reflects the Nationally Described Space Standards.
- 9.6.2 The application proposes 5x1bed, 6x2 bed and 3x3 bed flats, 14 in total, in addition to the re-provision of the 1 bed flat at first floor level within no. 469.

- 9.6.3 Policy 3.5 of the London Plan specifies that 1 bed flats should have a minimum floor area of 50sqm, 2 bed flats should have a minimum internal floor area of 61 square metres, with 2 bed 4 persons at 70sqm, 3b4p flats at 74 sqm or 3b6p flats at 86 sqm. All units have been measured and verified and are above the required London Plan standards for the respective units. All units would have useable, flexible and accessible layouts and all room sizes are acceptable with regards to living/diners and single and double bedrooms. All units would be dual aspect and a second stair core has been added to accommodate better individual access but also to facilitate the dual aspect units. All units would have adequate outlook, provision of natural light and good levels of privacy.
- 9.6.4 Additionally, the one bedroom flat to be re-provided at first floor level would be a like for like replacement of the existing 1 bed first floor flat. This arrangement is considered acceptable.

Housing Mix

- 9.6.5 DMD 3 and Policy 5 of the Core Strategy seeks new development to incorporate a mix of dwelling types and sizes to meet housing needs in the Borough.
- 9.6.6 The Council's dwelling mix ratios are as follows:
- 1 and 2-person flats - 20%
 - 2 bed flats - 15%
 - 3 bed houses - 45%
 - 4 + bed houses - 20%
- 9.6.7 The development provides the following dwelling mix:
- 5 x 1b2p (35%)
 - 6 x 2b (1 x.2b3p and 5 x 2b4p) (combined 42%)
 - 3 x 3b 4 or 5p (23%)
- 9.6.8 Having regard to the access requirements and the building envelope, 3 family units are considered to fit comfortably into the scheme, having regard to the confines of the site and the numbers of flats that could be reasonably accommodated at each respective floor. Additionally, 5 x 2 bed 4 person flats are proposed as part of the scheme which could feasibly accommodate smaller families, one of these units would also have direct access to the rear garden area.
- 9.6.9 It is therefore considered that the proposed mix of units and standard of accommodation are considered acceptable, having regard to adopted policies.

9.7 Private Amenity

- 9.7.1 Policy DMD9 specifies the requirements for private and communal amenity space for such developments.
- 9.7.2 Each of the proposed flats would be served by its own private amenity areas. The ground floor flats would benefit from their own policy compliant rear gardens along with front facing terraces. The remaining 10 flats would benefit from individual terraces and balconies all of which are policy compliant having

regard to Policy DMD9. A communal garden area of 100sqm in area is also proposed as part of the development.

- 9.7.3 The amenity provisions proposed are therefore considered acceptable, having regard to policy DMD9 of the DMD.

9.8 Traffic and Transportation

- 9.8.1 Policy DMD 45 seeks to minimise car parking and to promote sustainable transport options. The Council recognises that a flexible and balanced approach needs to be adopted to prevent excessive car parking provision while at the same time recognising that low on-site provision sometimes increases pressure on existing streets.

- 9.8.2 The proposed 12 car parking spaces for the additional 14 flats taking into account the moderate PTAL rating of 3 within the area and the reasonable access to public transport is acceptable. This would represent a parking ratio of 86%. It is acknowledged that the existing forecourt parking would be lost to accommodate the new undercroft access, however only 1x1 bedroom flat is to be re-provided over the undercroft and in this instance the lack of car parking is considered acceptable. Additionally, one disabled parking space and three electric bays should be provided and secured by appropriate condition. The proposed parking spaces meet the specified standard of 4.8m x 2.4m and the proposed layout of the spaces would allow vehicles to access and egress in a forward gear, which is considered acceptable.

- 9.8.3 With regards to access the property (front of No.469 Green Lanes) already has a dropped kerb that can be retained to accommodate vehicle crossover into the site. The proposed vehicle undercroft is 4.8m wide which is acceptable for a single vehicle exit and entrance point. This would not allow for 2 way vehicle movement, however it is considered that a priority waiting restriction can be put in place at the rearmost end of the undercroft, where the access road widens out to 6m in width. This could prioritise vehicles entering the site over those exiting to avoid any congestion for cars entering the site from Green Lanes. This can be dealt with via planning condition in addition to the exact details of the undercroft, retaining walls and treatment to the front of the site to allow for the access. Turning to pedestrian access, this measures approx. 1.50m wide, which is considered acceptable given site constraints, and is in line with Manual for Streets guidance. There is proposed pedestrian access throughout the site although it is noted that footways are not shown to the western side units. This is due to the lack of width available to maintain the 6.0m turning space for vehicles, however there is still an opportunity to provide overrun areas to improve the environment for pedestrians. These should be secured by an appropriate condition, however details will be clarified with Traffic and Transportation and verbally updated at Planning Committee.

- 9.8.4 Additionally, in line with the London Plan (March 2016), 20% of the total parking spaces should be provided as active electric vehicle (EV) charging points; with a further 20% (2.4 spaces) passive EV charging spaces. This level of provision should be distributed across the whole parking area.

- 9.8.5 From assessing the proposed Autotrack plans, vehicular access for the councils refuse truck and a fire engine can also be achieved. Refuse vehicles can access and turn within the site, confirmed by the Autotrack plans submitted in the TA for a large refuse vehicle. The refuse storage is located to the rear of

the site in the large turning head provided for the refuse vehicle. It is acknowledged that this is not the most efficient use of the hard standing, however it would allow easy and safe refuse collection.

- 9.8.6 The location and number of bins to the rear of the site within the turning head, and cycle storage in general is considered acceptable. Additionally, the site would provide a total of 26 bike spaces with a visitor space. The provision of 13 double stacked spaces is considered acceptable, although there should be one short stay space provided as well, however this could be secured by an appropriate condition.
- 9.8.7 Having regard to the Councils approach to mitigate against an increase in vehicular, cycle, and pedestrian trips generated by the development, a S106 contribution towards sustainable transport and healthy streets would be required. This would be used for resident's membership upon occupation for car club, oyster card, Cycling Campaign membership, and administration/promotional materials, and is based on the number of units and bedrooms.
- 9.8.8 Additionally, as mentioned above, there may be a requirement for "keep clear" markings across the access. These could be implemented via either S278 or by LBE Highways. These options, and the requirement, should be clearly set out in the S106 Agreement, however, will be further discussed with highway officers and verbally reported at Planning Committee.
- 9.8.9 Subject to the aforementioned conditions and S106 contributions towards highway works and health streets, the application is considered acceptable from a highway perspective.

9.9 Contamination

- 9.9.1 The revised NPPF refers to the need to enhance the natural and local environment by remediating and mitigating despoiled, degraded, derelict, contaminated and unstable land, where appropriate.
- 9.9.2 Having regard to past uses at the site, there is the potential for contamination at the site, which could pose a potential risk to human health. Environmental Health were consulted and have no objections to the proposed development subject to appropriate conditions to deal with the potential for contamination at the site as well as the control of dust and emissions, having regard to policies 5.3 and 7.14 of the London Plan, DMD64 of the DMD and CP32 of the Core Strategy.

9.10 s106 Contributions

- 9.10.1 Policies 8.1 and 8.2 of The London Plan (2016) seek to ensure that development proposals make adequate provision for both infrastructure and community facilities that directly relate to the development. Developers will be expected to meet the full cost of facilities required as a consequence of development and to contribute to resolving deficiencies where these would be made worse by development. In accordance with the S106 SPD and the comments received in respect of this application, the development should

Affordable Housing

- 9.10.2 Having regard to policies DMD1 of the Development Management Document and CP3 of the Core Strategy, as the site is proposing 10 or more units (14 and re provision of one existing unit) it should be complying with a borough wide target of achieving 40% affordable housing and a mix of tenures to reflect a borough wide target of 70% social rent and affordable rent and 30% Intermediate. This would reflect 6 units on this site as affordable housing.
- 9.10.3 The previous scheme ref. 15/05516/FUL was granted subject to a S106 legal agreement, which included an offsite contribution of £570,800 towards affordable housing following the assessment of a viability statement.
- 9.10.4 Since the previous planning permission was granted, the site has since been sold on and subsequently purchased by Andora Homes. As part of the application, the applicant has submitted a revised viability assessment, which concludes that the scheme would not be viable to contribute towards on-site affordable housing nor an off-site site contribution. The submitted report sets out that the residual land value is lower than the agreed benchmark value and therefore the landowner would not release the land for development.
- 9.10.5 Policy DMD1 states that affordable housing negotiations should take into account a number of issues including development viability. The viability assessment was independently reviewed and assessed by an independent and experienced commercial consultant, BNP Paribas, and concluded that the submitted appraisal is robust, and a financial contribution or on site provision of affordable housing would make the scheme unviable. This is due to an increase in build costs and changes to the site value benchmark. It has also been acknowledged that the existing clubhouse which is in a poor, dilapidated condition would require significant expenditure in order for the building to be in a lettable condition.
- 9.10.6 Although no affordable housing would be secured at this moment in time due to viability issues, to enable affordable housing to potentially be secured on site or through a financial contribution, an early and late stage review mechanism will be secured through a s106 agreement in line with the Mayor of London's Affordable Housing and Viability SPD. The approach to review mechanisms has been designed to assess changes in gross development values and build costs. The heads of terms for the S106 will include an early review that will be triggered if an agreed level of progress on implementing the permission has not been reached after two years of the planning permission being granted or as a time agreed with the LPA. The aim of an early review is to incentivise delivery of the development. A late stage review will be triggered once 75% homes are sold or let or at a point agreed by the LPA. The benefits of late stage reviews are that they are based on values achieved and costs incurred. The outcome of a late stage review will typically be a financial contribution towards off site affordable housing contribution.

Employment Skills

- 9.10.7 Having regard to the Councils Section 106 Supplementary Planning Document (November 2016), it is considered that appropriate regard towards Business, Employment and Skills are required.

- 9.10.8 Discussions are being undertaken with the relevant department, and an appropriate employment and skills strategy will be required and secured by S106, however appropriate details will be verbally reported at Planning Committee.

Education Contributions

- 9.10.9 Having regard to policy CP46 of the Core Strategy and the councils S106 SPD, this application would also be required to provide education contributions towards local schools in the area.
- 9.10.10 This application proposes 14 units which would equate to a contribution of £35,490 towards off site education contributions.

Other S106 Contributions/ Head of Terms

- 9.10.11 Following a review of the viability of the scheme in addition to the S106 allowance for education and Mayoral and borough CIL, a contribution towards healthy streets and sustainable transport is considered appropriate. However, the finite details of this will be agreed with highways officers and incorporated into the final S106 Agreement and further details of the amount required would be verbally reported at Committee.
- 9.10.12 Additionally, a S106 Management fee would be required, in line with the S106 SPD. The fee will be used for S106 administration, monitoring and management purposes only.

9.11 Sustainable Design and Construction

- 9.11.1 The revised National Planning Policy Framework (2019) places an increased emphasis on responding to climate change, having regard to long-term implications for flood risk, coastal change, water supply, biodiversity and landscape, and the risk of overheating from rising temperatures. New development can help to reduce greenhouse gas emissions, such as through its location, orientation and design. Any local requirements for the sustainability of buildings should reflect the Government's policy for national technical standards and increase the use and supply of renewable and low carbon energy and heat. The NPPF states that even small-scale projects provide a valuable contribution to cutting greenhouse gas emissions.
- 9.11.2 Policy DMD49 states that all new development must achieve the highest sustainable design and construction standards having regard to technical feasibility and economic viability. An energy statement in accordance with Policies DMD49 and 51 is required to demonstrate how the development has engaged with the energy hierarchy to maximise energy efficiency.
- 9.11.3 The London Plan adopts a presumption that all developments will meet carbon dioxide emission reductions that will improve upon 2010 Building Regulations, leading to zero carbon residential buildings from 2016. Policy 5.2 establishes a target for 2013 to be a 35% improvement over Part L of current Building Regulations
- 9.11.4 It was noted that an Energy and Sustainability Statement (dated 12 November 2015) has been submitted, however as this was prepared in 2015,

it is considered out of date and therefore an appropriate condition would be attached to secure a revised Energy Statement in light of the updates to the National Planning Policy Framework.

9.11.5 Policy DMD52 all major development should connect to or contribute towards existing or planned decentralised energy networks (DEN) supplied by low or zero carbon energy. Proposals for major development which produce heat/ and or energy should contribute to the supply of decentralised energy networks unless it can be demonstrated that this is not technically feasible or economically viable. It is noted that there may be the possibility for the site to connect to a planned decentralised energy network such as Ladderswood or Meridian Water. However, internal discussions are being undertaken with the relevant department and an update will be verbally reported at Planning Committee.

9.11.6 Policy DMD55 requires all development to maximise the use of roof and vertical surfaces for Low and Zero Carbon Technology / Living Walls / Green Roofs. A condition will be attached to any permission.

9.11.7 Policy DMD58 (Water Efficiency) expects new residential development, including new build and conversions, will be required to achieve as a minimum water use of under 105 litres per person per day. This could be secured by an appropriate condition.

9.12 Biodiversity and Trees

9.12.1 The revised National Planning Policy Framework (2019) has referred to biodiversity and specifies that all new development should be planned to avoid increased vulnerability to the range of impacts arising from climate change. When new development is brought forward in areas which are vulnerable, care should be taken to ensure that risks can be managed through suitable adaptation measures, including through the planning of green infrastructure. Additionally, it states the need to enhance the local environment by minimising the impacts on and providing net gains for biodiversity, including by establishing coherent ecological networks that are more resilient to current and future pressures.

9.12.2 Policy CP36 of the Core Strategy states that the Council will seek to protect, enhance, restore or add to biodiversity interests within the Borough, including parks, playing fields and other sports spaces, green corridors, waterways, sites, habitats and species identified at a European, national, London or local level as being of importance for nature conservation. The site is located within a wildlife corridor and policy DMD76 of the DMD states that development that falls within or abuts a wildlife corridor will only be permitted if the proposals protect and enhance the corridor.

9.12.3 It is noted that an Ecological Report (July 2015) has been provided which includes an extended Phase 1 Habitat Survey. The survey specifically refers to the need for bat and reptile surveys, as well as an Ecological Mitigation Plan to increase habitat opportunities across the site for both protected and non-protected species. The Ecological Survey refers to the incorporation of underground bumble bee boxes, bird boxes, insect hotels, a stag beetle logger at the site as well as the requirement for bat surveys and a full reptile survey. The proposal would include the demolition of an existing building and snooker hall. There are suspected bat roosts of unknown type and bats could

also be using the west of the site to commute along the railway line. It is considered that an appropriate condition is required to include a revised Ecological Report and the specified bat and reptile surveys to safeguard the existing wildlife at the site. An appropriate condition would also be attached to secure a green/brown roof to enhance the biodiversity at the site.

9.12.4 The Councils tree officer has stated that there are a large number of trees on and off-site that may provide constraints to this development. Policies DMD80 and DMD81 of the Development Management Document refer to trees on development sites. It is noted that these matters were not identified within the previously granted scheme, however trees are a significant material consideration to all development applications and thus appropriate conditions have been attached relating to an Arboricultural Impact Assessment to standard BS5837:2012 and landscaping to provide adequate greenery and safeguard existing trees on site where applicable.

9.13 Sustainable Drainage System (SuDS)

9.13.1 The revised NPPF states that all major developments should incorporate sustainable drainage systems unless there is clear evidence that this would be inappropriate. The systems used should:

- a) Take account of advice from the lead local flood authority;
- b) Have appropriate proposed minimum operational standards;
- c) Have maintenance arrangements in place to ensure an acceptable standard of operation for the lifetime of the development; and
- d) Where possible, provide multifunctional benefits.

9.13.2 Policy DMD61 of the DMD specifies that a drainage strategy would be required for all developments to demonstrate how proposed measures manage surface water as close to its source as possible and follow the drainage hierarchy in the London Plan. All development must maximise the use of, and where possible, retrofit Sustainable Drainage Systems (SuDS).

9.13.3 The Councils SuDS officer provided comments relating to drainage and further details have been secured by appropriate conditions, having regard to policies CP28 of the Core Strategy, DMD60 and DMD61 of the DMD and 5.12 & 5.13 of the London Plan as well as the advice contained within the NPPF.

9.14 CIL

9.14.1 The development shall pay the following CIL contributions upon commencement of development. The size of the proposed development would be liable to a Community Infrastructure Levy contribution as the size exceeds 100 sq.m. The net gain of the new created floor area is 912 sq.m, inclusive of the 14 units and the communal staircase area.

Mayoral CIL

9.14.2 The Mayoral CIL is collected by the Council on behalf of the Mayor of London. The amount that is sought for the scheme is calculated on the net increase of gross internal floor area multiplied by the Outer London weight of £60 together with a monthly indexation figure. It is noted as of the 1st of April 2019 Mayoral CIL has increased to £60/m².

- 9.14.3 This would result in a Mayoral CIL contribution of $912 \text{ sq.m} \times £60 = £54,720 \times \frac{336}{223}$ (BCIS CIL Index Formula) = £82,448.07.
- 9.14.4 On April 2016, the Council introduced its own CIL. The money collected from the levy (Regulation 123 Infrastructure List) will fund rail and causeway infrastructure for Meridian Water.
- 9.14.5 This would result in a Borough CIL contribution of $912 \text{ sq.m} \times £120 = £109,440 \times \frac{283}{336}$ (BCIS CIL Index Formula) = £92,177.14.

7. Conclusion

- 7.1 The proposed development would have an acceptable impact to the character and appearance of the site and surrounding area as well as an acceptable relationship with adjoining neighbours. It would provide for 3 additional family units and 14 additional residential units in a relatively accessible part of the borough.
- 7.2 The proposed development would not be detrimental to neighbouring amenity or have an unacceptable impact on highway function and safety. The proposed development would improve biodiversity and sustainable construction to mitigate and adapt towards climate change.
- 7.3 Subject to appropriate conditions and the completion of a S106 Legal Agreement which will include review mechanisms to capture any potential increase in affordable housing on the site, it is recommended that planning permission is granted.

APCAR SMITH PLANNING
Chartered Town Planning Consultants

**EXPLANATORY BACKGROUND
DOCUMENT**

**465-469 GREEN LANES
LONDON
N13 4BS**

JANUARY 2019

Our Ref: CA/3103

Kinetic House, Theobald Street,
Borehamwood, Hertfordshire WD6 4PJ
Tel: 020 8387 1387
E-Mail: enquiries@apcarsmithplanning.co.uk



This Statement accompanies a planning application by Andora Homes Limited for the redevelopment of the site to provide 15 residential units. This includes the re-provision of an existing 1 bedroom flat fronting Green Lanes with the remainder of the development being in two blocks. Block 1 is a three storey block of 3 flats (1 x 1 bed, 1 x 2 bed and 1 x 3 bed). Block 2 is a part three/part four storey block of 11 flats (4 x 1 bed, 5 x 2 bed and 2 x 3 bed units). The redevelopment incorporates the construction of a new access way off Green Lanes, off-street car parking, with bicycle and refuse/recycle storage in a detached building.

The application is identical to permission Ref: 15/05516/FUL, the decision letter for which is dated 13 September 2017. That permission is subject to a Section 106 Agreement dated 13 August 2018.

The Agreement included for an affordable housing contribution split into two parts. Part 1 was for the sum of £285,400 which is due to be paid upon commencement of development. Part 2, also for £285,400, is due to be paid on occupation of the 5th dwelling. The Agreement included a provision for a Viability Appraisal Review to be submitted once 50% of the dwellings have been sold. Any additional profit identified by this Review is to be shared between the Council and the owner with 60% payable to the Council towards the affordable housing contribution. No more than 65% of the dwellings can be occupied until the Review has been concluded and the Additional Affordable Housing Contribution, if required, has been paid to the Council.

In addition to these affordable housing contributions the Section 106 Agreement included a requirement for a contribution of £35,490 towards education, £43,480 towards transport initiatives and a monitoring fee of £32,490. The education and transport contributions, like Part 1 of the affordable housing contribution, are required to be paid upon commencement of development.

This previous application had been submitted by the former owner of the site, Kuros Consultants Limited. Prior to the completion of the Section 106 Agreement they had exchanged to sell the site to a developer who was intending to implement the planning permission. Provided is a letter dated 10 January 2019 from Kuros Consultants Limited. Their letter explains that, unfortunately, the intended purchaser pulled out of the purchase, primarily as a result of the financial contribution towards affordable housing which was considered to render the development unviable.

The site has now been sold to Andora Homes Limited (with the exception of that part of the Green Lanes frontage not affected by the proposals

themselves – the pair of semi-detached properties comprising 465/467 Green Lanes). This application is submitted by Andora Homes. It is for precisely the same form of development as previously approved with the same plans and supporting statements. The only difference in terms of submitted documentation is this statement and a fresh Viability Appraisal. The intended purpose of this application is to seek to have the affordable housing contribution reconsidered on the basis of the Viability Assessment that has now been undertaken by Bidwells.

The plans on which basis the application is submitted are as listed on the grant of planning permission and are as follows:

- Drwg No 983/50 Site Location Plan
- Drwg No 983/51 Site Plan as Existing
- Drwg No 983/52 Rev A Site Plan as Proposed
- Drwg No 983/53A Block Plan as Proposed
- Drwg No 983/54 Rev A Block 1 Plans and Elevations as Proposed.
- Drwg No 983/55 Rev A Block 2 – Plans as Proposed
- Drwg No 983/56 Rev A Block 2 Elevations as Proposed
- Drwg No 983/57 Rev B Site Section AA and Elevation as Existing and Proposed
- Drwg No 983/58 Rev A Street Elevation BB as Existing and Proposed
- Drwg No 983/59 Existing Site Photographs
- Drwg No 983/60A Visualisation 1
- Drwg No 983/61 Rev A Visualisation 2
- Drwg No 983/62 Rev A Aerial Views as Existing and Proposed
- Drwg No 983/63 Bicycle Store and Refuse/Recycling Store Details
- Drwg No 983/64 Rev A Site Elevation CC as Proposed
- Drwg No 983/65 Visualisation 2
- Drwg No 983/66 Ground and First Floor Plans as Existing
- Drwg No 983/67 469 Green Lanes as Proposed

A typographical error has been noticed on the decision letter. This refers to Drwg No 983/59 Rev A. However no Rev A was ever submitted.

This application is accompanied by the following supporting statements which were also considered by the Council as part of their determination of Application Ref 15/05516/FUL:

- Design and Access Statement

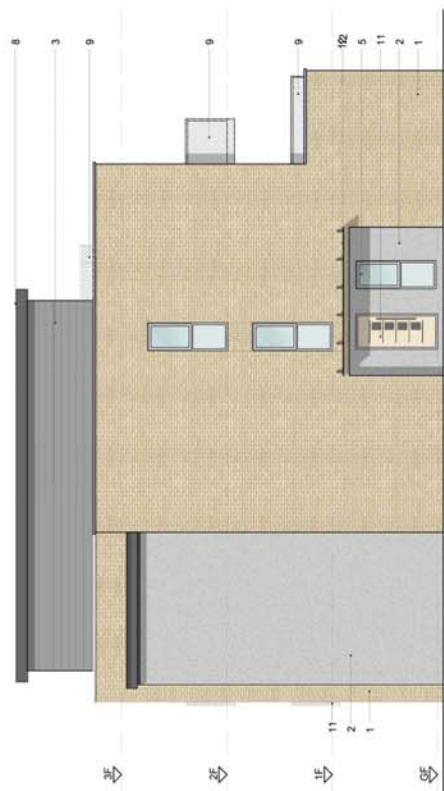
- Ecological Appraisal
- Energy and Sustainability Statement
- Lifetimes Homes Statement
- Noise and Vibration Assessment
- Outline Drainage Strategy
- Planning Statement
- Refurbishment Demolition Survey
- Site Waste Management Plan
- Transport Statement

The previously submitted reports in respect of viability are not included with this application. Instead the Applicant has instructed Bidwells to review matters afresh regarding viability. Their statement, assessing the economic viability of the extant scheme, is submitted with this application. This is the only document that differs from those previously submitted and found to be acceptable by the Local Authority when planning permission was granted.

As can be seen Bidwells demonstrate that the extant permission generates a negative residual land value and concludes that it cannot be considered viable in the current market. As they refer even when the affordable housing contribution is reduced to zero the extant scheme can only generate a residual land value of £142,938. This remains £857,062 below the Viability Benchmark Value previously agreed at £1 million. As Bidwells refer the Applicant is prepared to absorb this deficit in anticipation that market conditions will improve sufficiently over the lifetime of the project to enable the scheme to generate the target profit of 20% on GDV. They therefore conclude that a zero affordable housing contribution satisfies the test set out in local and national planning policy and should therefore be considered acceptable to the Local Authority.

To enable the matter of viability to be completely reconsidered it is necessary to resubmit this full planning application with all documentation once again and to pay the full application fee to the Local Authority. However with the exception of matters in respect of affordable housing contribution there has been no change. There has been no change on the ground – either on the application site or in the surrounding area. Nor has there been any change in planning policy other than the revised National Planning Policy Framework which was published in July 2018. The latter does not affect matters in respect of affordable housing/affordable housing contributions other than for non-major applications (ie; applications of less than 10 dwellings) and therefore this revised policy document does not affect matters relevant to this planning application.

Given that Bidwells demonstrate that the proposed development will not be viable with any contribution towards affordable housing it is hoped that the Local Authority will once again grant planning permission but with no requirement for there to be a financial contribution towards affordable housing.



BLOCK 2 - Elevation B as Proposed



BLOCK 2 - Elevation D as Proposed



BLOCK 2 - Elevation A as Proposed

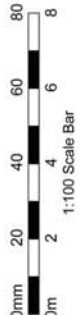


BLOCK 2 - Elevation C as Proposed

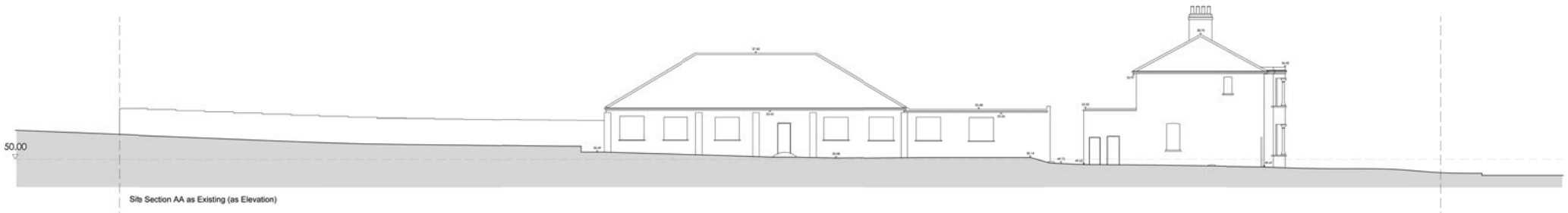


- Yellow facing brickwork
- Glass balustrade
- K Rend external render, colour: off-white
- Composite light grey powder-coated aluminium insulated panel
- White uPVC framed window with top hung opener
- White uPVC framed glazed door
- White powder-coated aluminium framed glazed bi-folding door
- Grey aluminium roof to penthouse

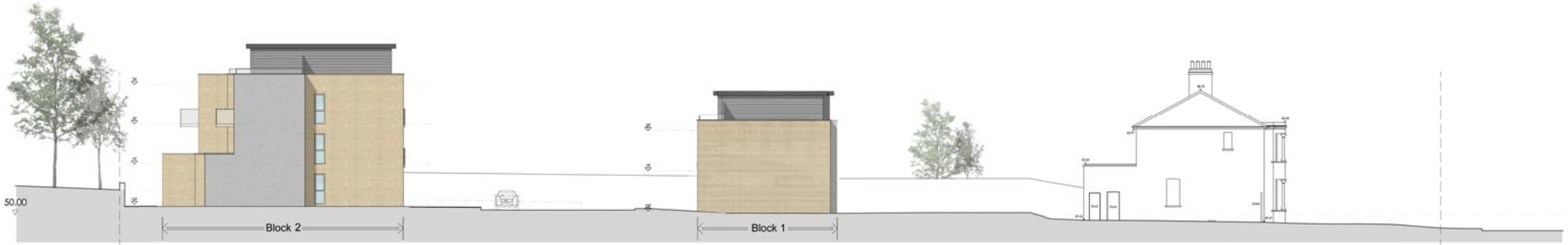
- MATERIALS KEY:**
- 1 Yellow facing brickwork in cavity construction
 - 2 K Rend external render, colour: off-white
 - 3 Cedar weatherboard, colour: light grey
 - 4 Composite light grey powder-coated aluminium insulated panel
 - 5 White uPVC framed window with top hung opener
 - 6 White uPVC framed glazed door
 - 7 White powder-coated aluminium framed glazed bi-folding door
 - 8 Grey aluminium roof to penthouse
 - 9 Metal balcony with glass balustrade
 - 10 Glass Juliette balcony with metal handrail
 - 11 Timber painted entrance door
 - 12 Glass/Mesa entrance canopy
 - 13 Obscure glass screening panel



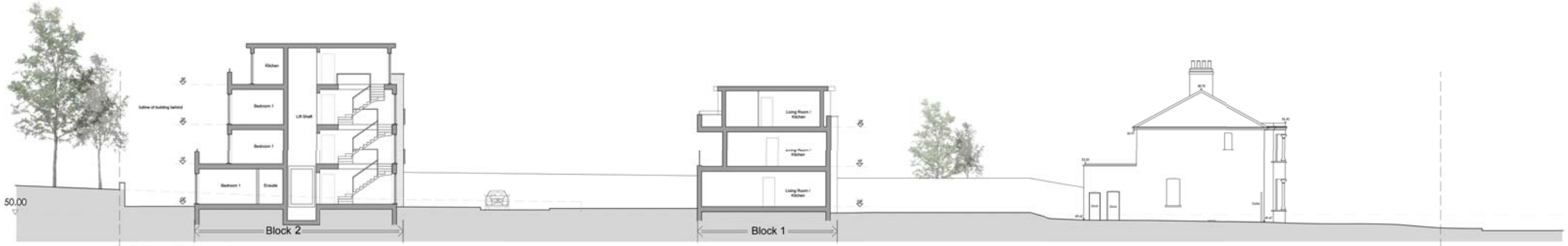
	221 East Barnet Road, Barnet, Herts EN4 6GS Tel: (020) 8448 5100 Fax: (020) 8448 5170	Client: Kuros Consultants	Site: 465-469 Green Lanes London N13 4BS	Drawing Title: Block 2 - Elevations as Proposed	Scale: 1:100 @ A2
	Date: June 2016	Drawn by: 983/56	Checked by: A	Project No.: 983/56	Date: June 2016



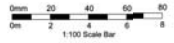
Site Section AA as Existing (as Elevation)



Site Section AA as Proposed (as Elevation)

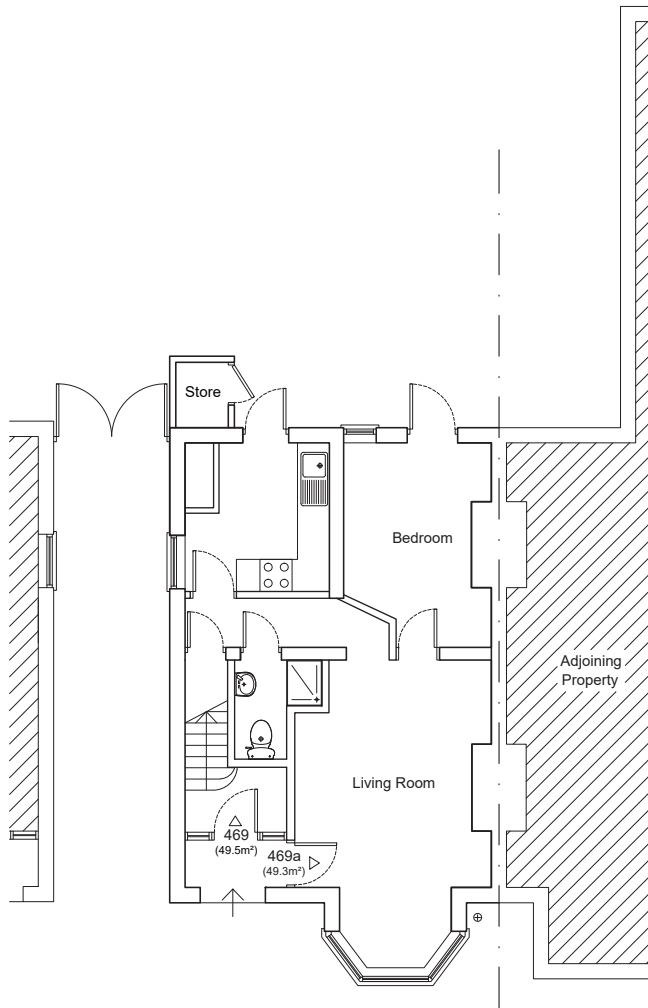


Site Section AA as Proposed

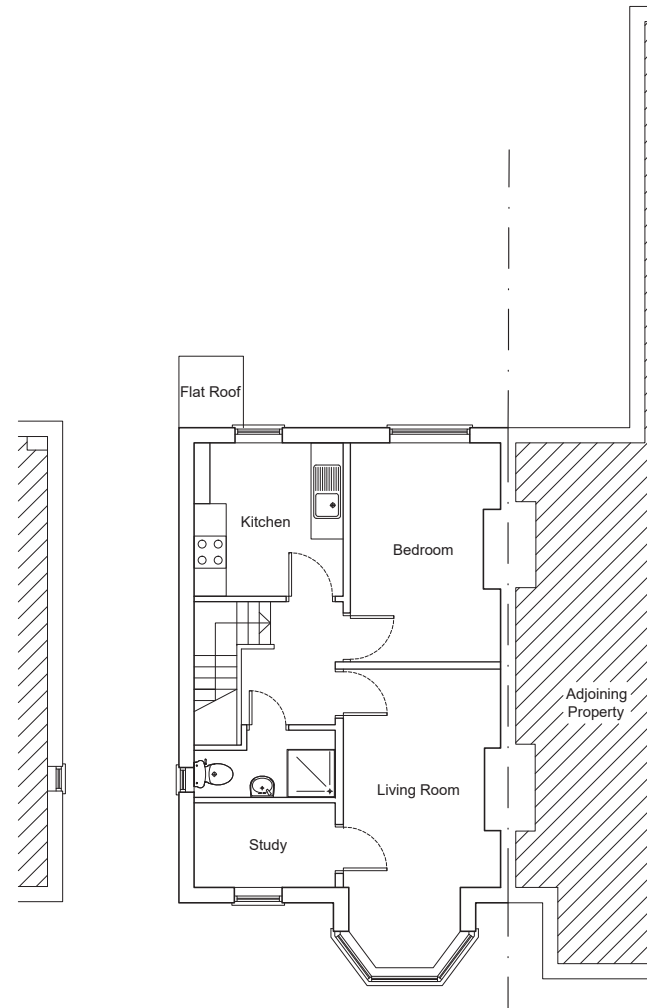


No scaled dimensions to be taken from this drawing. All dimensions to be site checked.

<p>CCA 221 East Barnet Road, Barnet, Herts EN4 8DS Tel: 020 8449 5100 Fax: 020 8449 5170</p>	<p>Site 465-469 Green Lanes London N13 4BS</p>	<p>Title Site Section AA & Elevation as Existing and Proposed</p>
	<p>Client Kuros Consultants</p>	<p>Scale: 1:100 @ A0 Date: June 2016</p>



Ground Floor Plan as Existing



First Floor Plan as Existing



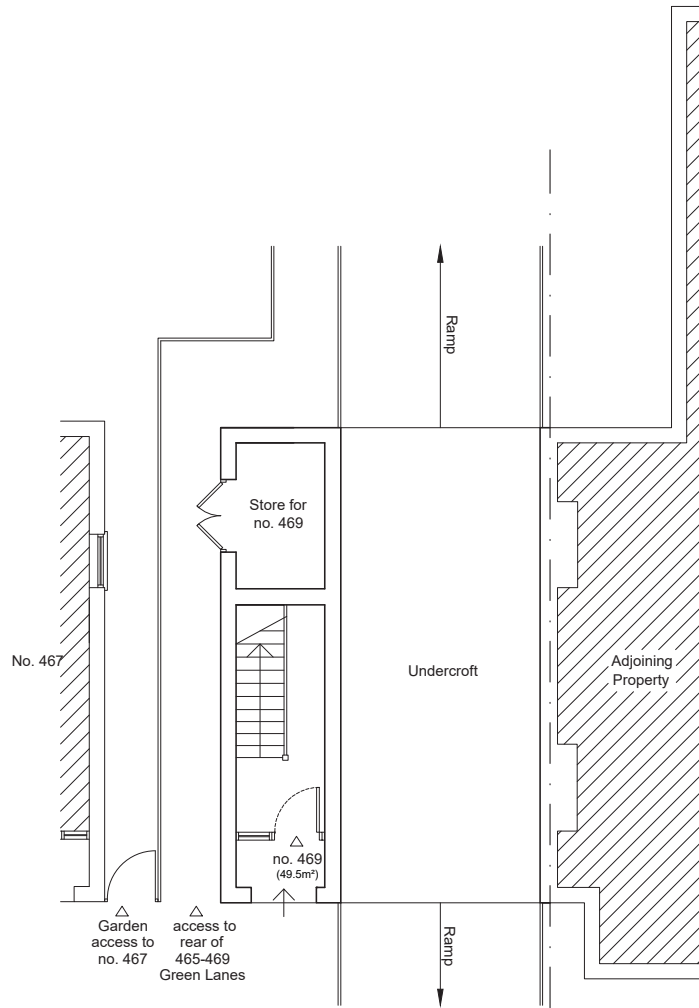
221 East Barnet Road,
Barnet, Herts EN4 8QS
Tel: 020 8449 5100
Fax: 020 8449 5170

Client:
Kuros Consultants

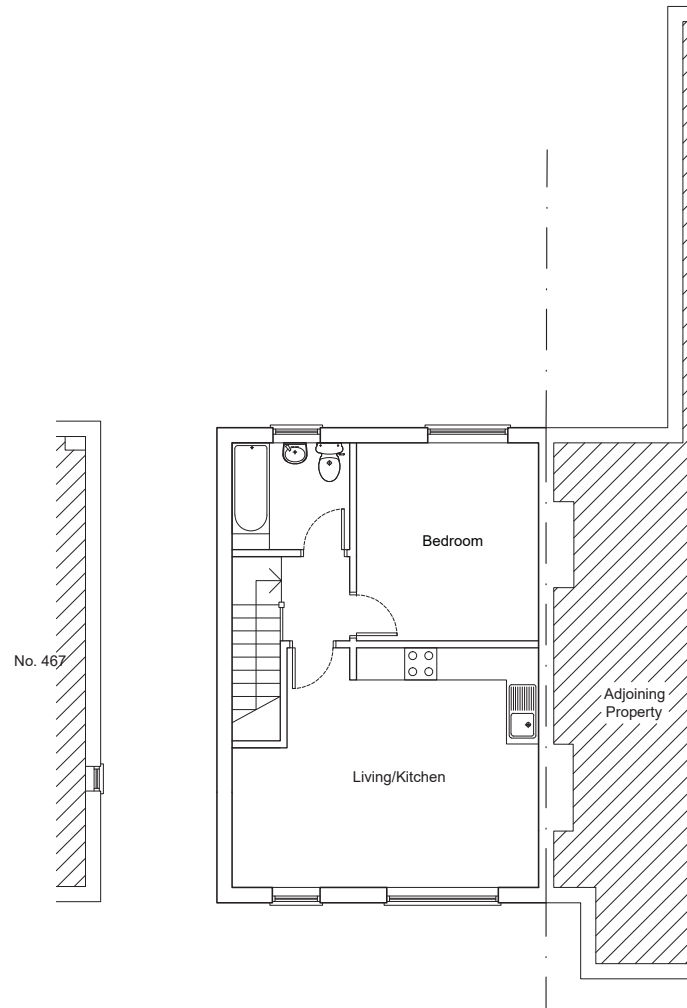
Site:
465-469 Green Lanes
London
N13 4BS

Drawing Title:
469 Green Lanes
as Existing

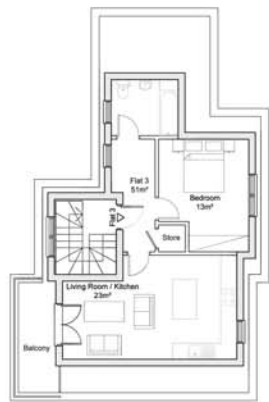
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Date: June 2016	
Dr. No. 983/66	Rev. #



Ground Floor Plan as Proposed



First Floor Plan as Proposed



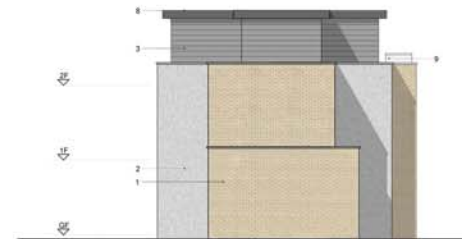
Second Floor Plan as Proposed



Roof Plan as Proposed



BLOCK 1 - Elevation A as Proposed



BLOCK 1 - Elevation B as Proposed



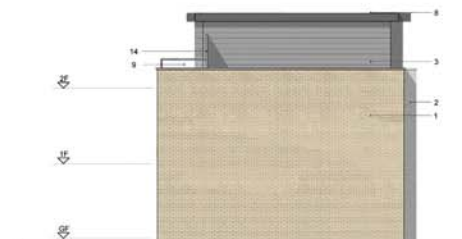
Ground Floor Plan as Proposed



First Floor Plan as Proposed



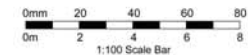
BLOCK 1 - Elevation C as Proposed



BLOCK 1 - Elevation D as Proposed

MATERIAL KEY:

1 Yellow facing brickwork in cavity construction	9 Metal balcony with glass balustrade	Yellow facing brickwork
2 K Rend external render, colour: off-white	10 Glass Juliette balcony with metal handrail	Glass balustrade
3 Cebral weatherboard, colour: light grey	11 Timber painted entrance door	Cebral weatherboard
4 Composite light grey powder-coated aluminium insulated panel	12 Glass/Metal entrance canopy	K Rend external render, colour: off-white
5 White uPVC framed window with top hung operable	13 Obscure glass screening panel	Obscure glass screening panel
6 White uPVC framed glazed door		
7 White powder-coated aluminium framed glazed bi-folding door		
8 Grey aluminium roof to perthouse		



No scaled dimensions to be taken from this drawing. All dimensions to be site checked.

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Site: 465-469 Green Lanes
 London N13 4BS

Title: Block 1 - Plans and Elevations as Proposed

Client: Kuros Consultants

Scale: 1:100 @ A1
 Date: June 2016

Drawn: []
 Dwg. No: 983/54
 Rev: A

- KEY:
- Landscaping
 - Grass
 - Parking - Tarmac
 - Patio
 - Path - Paving slabs
 - Site Boundary

Refuse/Recycling Store for 14no. flats:
 3 x 1100ltr. refuse bins
 1 x 1280ltr. recycling bin



No scaled dimensions to be taken from this drawing. All dimensions to be site checked.

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Site:
 465-469 Green Lanes
 London
 N13 4BS

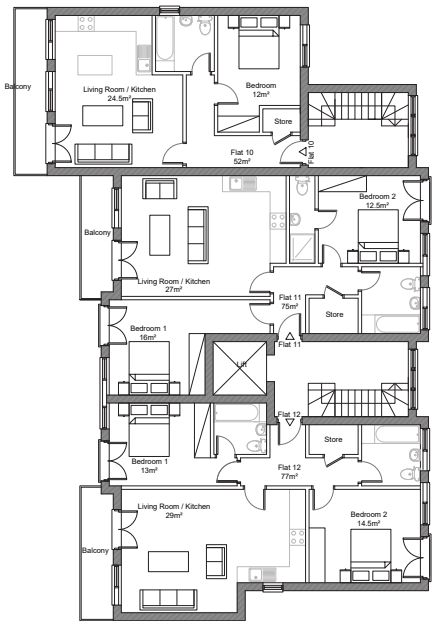
Title:
 Site Plan as Proposed

Client:
 Kuros Consultants

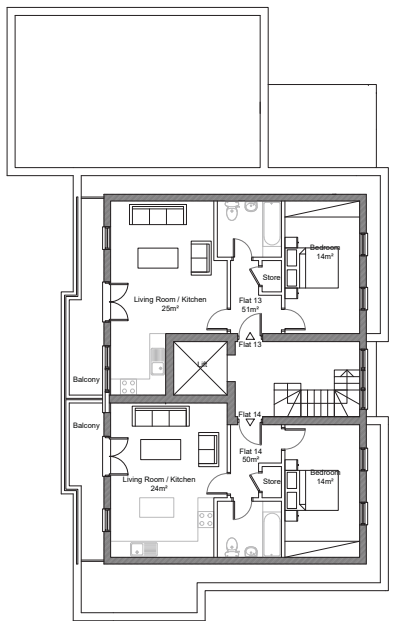
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 Date: June 2016

Drawn:

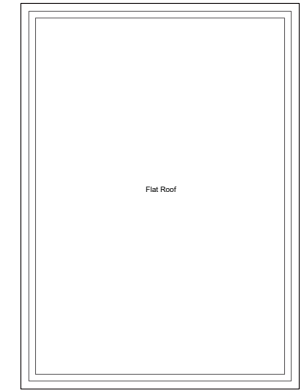
Dr. No.
 983/52
 Rev.
 A



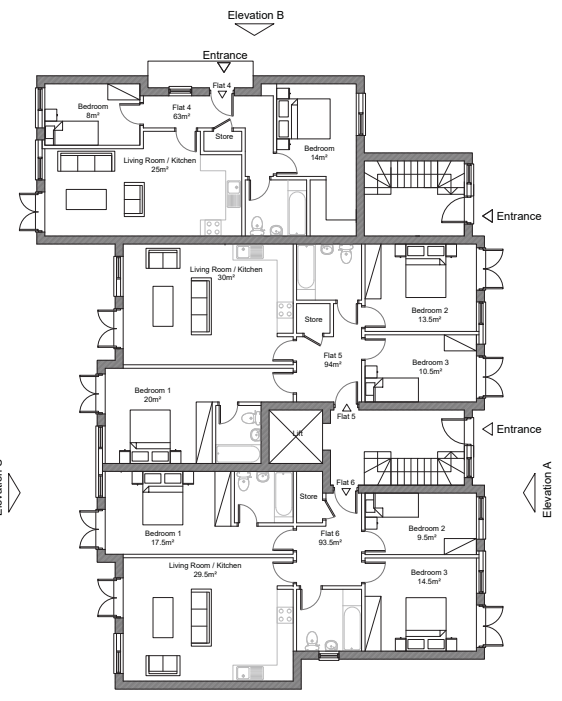
Second Floor Plan as Proposed



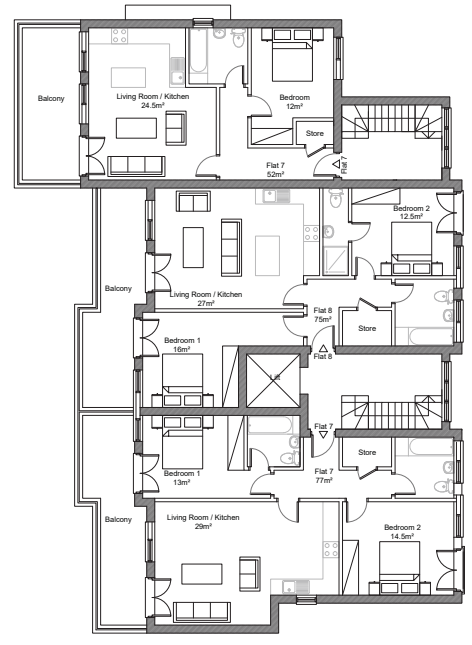
Third Floor Plan as Proposed



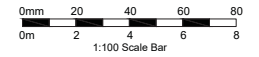
Roof Plan as Proposed



Ground Floor Plan as Proposed



First Floor Plan as Proposed



No scaled dimensions to be taken from this drawing. All dimensions to be site checked.

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Site: 465-469 Green Lanes
 London
 N13 4BS


Title: Block 2 - Plans as Proposed

Client: Kuros Consultants

Scale: 1:100 @ A1
 Date: June 2016

Drawn: []
 Drg. No. 983/55
 Rev. A



	221 East Barnet Road, Barnet, Herts EN4 8QS Tel: 020 8449 5100 Fax: 020 8449 5170	Client: Kuros Consultants	Site: 465-469 Green Lanes London N13 4BS	Drawing Title: Visualisation 2	Scale: Not to Scale
					Date: June 2016
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Drg. No. 983/65	Rev. #				